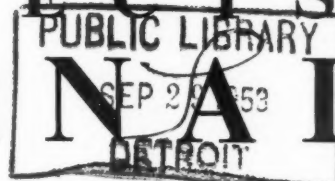


# THE ARCHITECTS' JOURNAL



## standard contents

every issue does not necessarily contain all these contents, but they are the regular features which continually recur.

## NEWS and COMMENT

Diary

News

Astragal's Notes and Topics

Letters

Societies and Institutions

## TECHNICAL SECTION

Information Sheets

Information Centre

Current Technique

Questions and Answers

Prices

The Industry

## PHYSICAL PLANNING

### SUPPLEMENT

## CURRENT BUILDINGS

## HOUSING STATISTICS

Architectural Appointments  
Wanted and Vacant

No. 3054]

[Vol. 118

THE ARCHITECTURAL PRESS

11 and 13, Queen Anne's Gate, Westminster,  
S.W.1. 'Phone: Whitehall 0611

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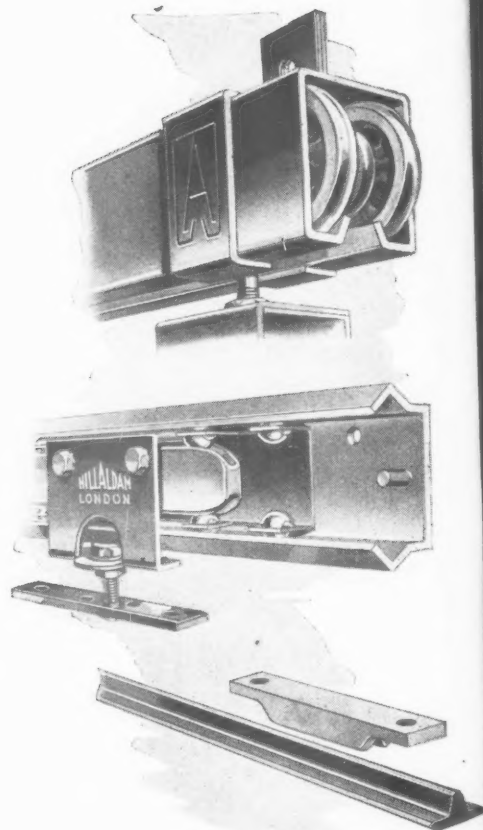
★ A glossary of abbreviations of Government Departments and Societies and Committees of all kinds, together with their full address and telephone numbers. The glossary is published in two parts—A to Ie one week, Ig to Z the next. In all cases where the town is not mentioned the word LONDON is implicit in the address.

IGE	Institution of Gas Engineers. 17, Grosvenor Crescent, S.W.1	Sloane 8266
IHVE	Institution of Heating and Ventilating Engineers. 75, Eaton Place, S.W.1.	Sloane 3158/1601
IIBD	Incorporated Institute of British Decorators. Drayton House, Gordon Street, W.C.1.	Euston 2450 Museum 1783
ILA	Institute of Landscape Architects. 12, Gower Street, W.C.1.	
I of Arb	Institute of Arbitrators. 35/37, Hastings House, 10, Norfolk Street, Strand, W.C.2.	Temple Bar 4071
IOB	Institute of Builders. 48, Bedford Square, W.C.1.	Museum 7197/5176
IR	Institute of Refrigeration. Dalmeny House, Monument Street, E.C.3.	Avenue 6851
IRA	Institute of Registered Architects. 47, Victoria Street, S.W.1.	Abbey 6172
ISE	Institution of Structural Engineers. 11, Upper Belgrave Street, S.W.1.	Sloane 7128
IWA	Inland Waterways Association. 14, Great James' Street, W.C.2.	Chancery 7718
LIDC	Lead Industries Development Council. Eagle House, Jermyn Street, S.W.1.	Whitehall 7264/4175
LMBA	London Master Builders' Association. 47, Bedford Square, W.C.1.	Museum 3891
MARS	Modern Architectural Research Group (English Branch of CIAM) Secretary: Gontran Goulden, Building Centre, 26, Store Street, W.C.1.	Museum 5400
MOA	Ministry of Agriculture and Fisheries. 55, Whitehall, S.W.1.	Whitehall 3400
MOE	Ministry of Education. Curzon Street House, Curzon Street, W.1.	Mayfair 9400
MOH	Ministry of Health. 23, Saville Row, W.1.	Regent 8411
MOHLG	Ministry of Housing and Local Government. Whitehall, S.W.1.	Whitehall 4300
MOLNS	Ministry of Labour and National Service, 8, St. James' Square, S.W.1.	Whitehall 6200
MOS	Ministry of Supply. Shell Mex House, Victoria Embankment, W.C.	Gerrard 6933
MOT	Ministry of Transport. Berkeley Square House, Berkeley Square, W.1.	Mayfair 9494
MOW	Ministry of Works. Lambeth Bridge House, S.E.1.	Reliance 7611
NAMMC	Natural Asphalt Mine-Owners and Manufacturers Council. 94-98, Petty France, S.W.1.	Abbey 1010
NAS	National Association of Shopfitters. 9, Victoria Street, S.W.1.	Abbey 4813
NBR	National Buildings Record. 37, Onslow Gardens, S.W.7.	Kensington 8161
NCBMP	National Council of Building Material Producers, 10, Princes Street, S.W.1.	Abbey 5111
NFBTE	National Federation of Building Trades Employers. 82, New Cavendish Street, W.1.	Langham 4041/4054
NFBTO	National Federation of Building Trades Operatives, Federal House, Cedars Road, Clapham, S.W.4.	Macaulay 4451
NFHS	National Federation of Housing Societies. 13, Suffolk St., S.W.1.	Whitehall 1693
NHBRC	National House Builders Registration Council. 82, New Cavendish Street, W.1.	Langham 4341 Molesley 1380
NPL	National Physical Laboratory. Head Office, Teddington	City 1476
NSA	National Sawmilling Association. 14, New Bridge Street, E.C.4.	
NSAS	National Smoke Abatement Society. Chandos House, Buckingham Gate, S.W.1.	Abbey 1359
NT	National Trust for Places of Historic Interest or Natural Beauty. 42, Queen Anne's Gate, S.W.1.	Whitehall 0211
PEP	Political and Economic Planning. 16, Queen Anne's Gate, S.W.1.	Whitehall 7245
RCA	Reinforced Concrete Association. 94, Petty France, S.W.1.	Abbey 4504
RIAS	Royal Incorporation of Architects in Scotland. 15, Rutland Square, Edinburgh 20396	Edinburgh 20396
RIBA	Royal Institute of British Architects. 66, Portland Place, W.1.	Langham 5721
RICS	Royal Institution of Chartered Surveyors. 12, Great George St., S.W.1.	Whitehall 5322/9242
RFAC	Royal Fine Art Commission. 22A, Queen Anne's Gate, S.W.1.	Whitehall 3935
RS	Royal Society. Burlington House, Piccadilly, W.1.	Regent 3335
RSA	Royal Society of Arts. 6, John Adam Street, W.C.2.	Trafalgar 2366
RSI	Royal Sanitary Institute. 90, Buckingham Palace Road, S.W.1.	Sloane 5134
RIB	Rural Industries Bureau. 35, Camp Road, Wimbledon, S.W.19.	Wimbledon 5101
SBPM	Society of British Paint Manufacturers. Grosvenor Gardens House, Grosvenor Gardens, S.W.1.	Victoria 2186
SCR	Society for Cultural Relations with the USSR. 14, Kensington Square, London, W.8.	Western 1571 Abbey 7244
SE	Society of Engineers. 17, Victoria Street, Westminster, S.W.1.	
SFMA	School Furniture Manufacturers' Association. 30, Cornhill, London, E.C.3.	Mansion House 3921
SIA	Structural Insulation Association. 32, Queen Anne Street, W.1.	Langham 7616
SIA	Society of Industrial Artists. 7, Woburn Square, W.C.1.	Langham 1984
SNHTPC	Scottish National Housing. Town Planning Council. Hon. Sec., Robert Pollock, Town Clerk, Rutherglen.	
SPAB	Society for the Protection of Ancient Buildings. 55, Great Ormond Street, W.C.1.	Holborn 2646
TCPA	Town and Country Planning Association. 28, King Street, Covent Garden, W.C.2.	Temple Bar 5006
TDA	Timber Development Association. 21, College Hill, E.C.4.	City 4771
TPI	Town Planning Institute. 18, Ashley Place, S.W.1.	Victoria 8815
TTF	Timber Trades Federation. 75, Cannon Street, E.C.4.	City 5051
WDC	War Damage Commission. 6, Carlton House Terrace, S.W.1.	Whitehall 4341
ZDA	Zinc Development Association. Lincoln House, Turl Street, Oxford.	Oxford 47988

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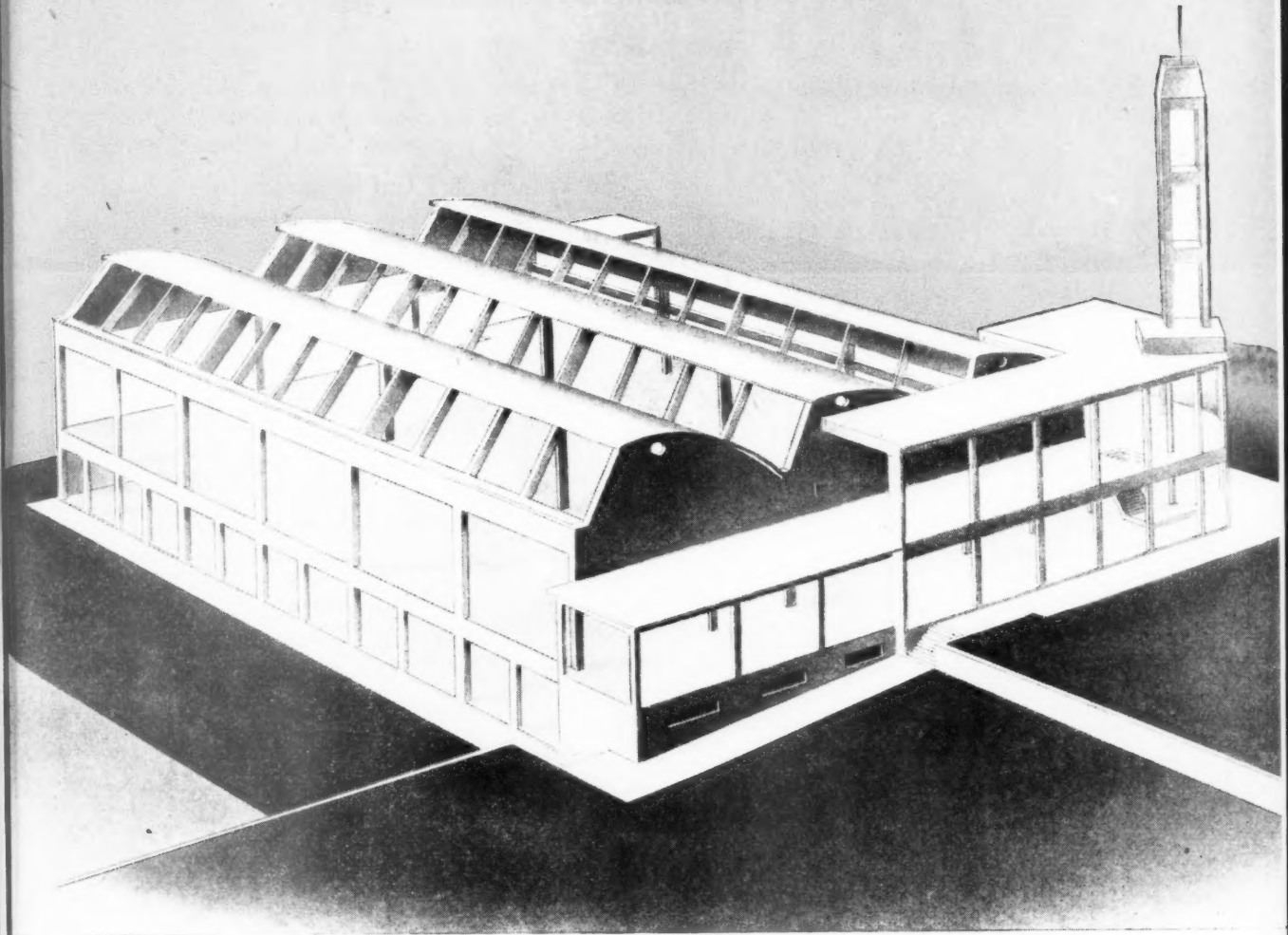
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THE ARCHITECTS' JOURNAL for September 10, 1953

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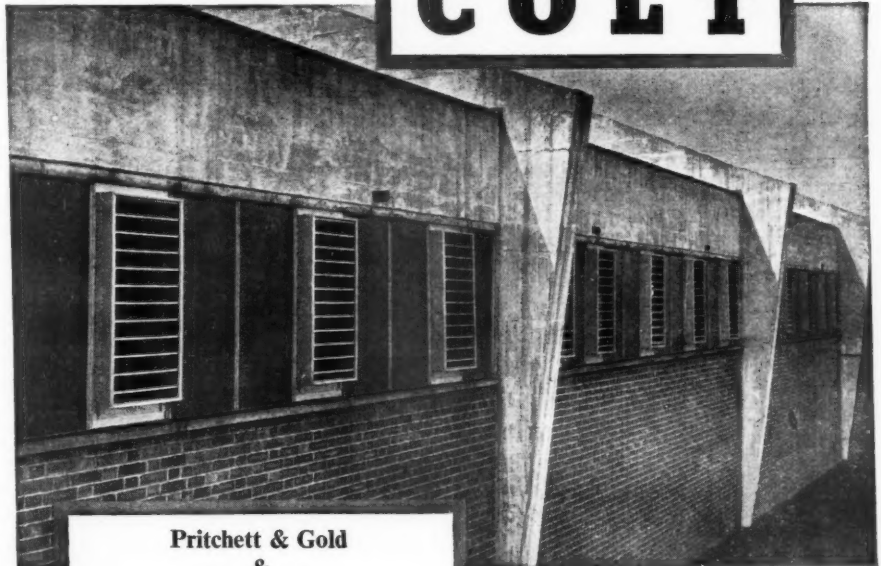


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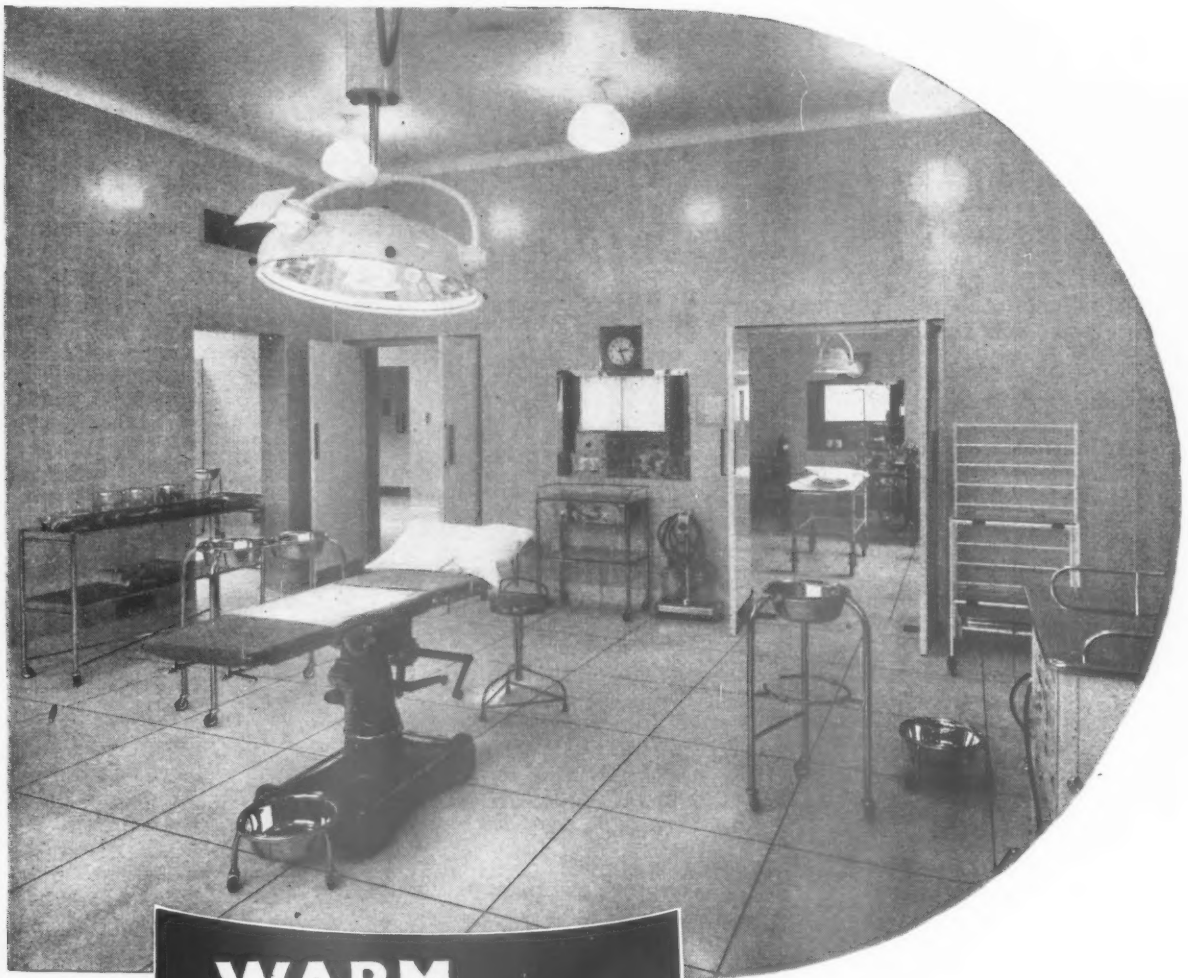
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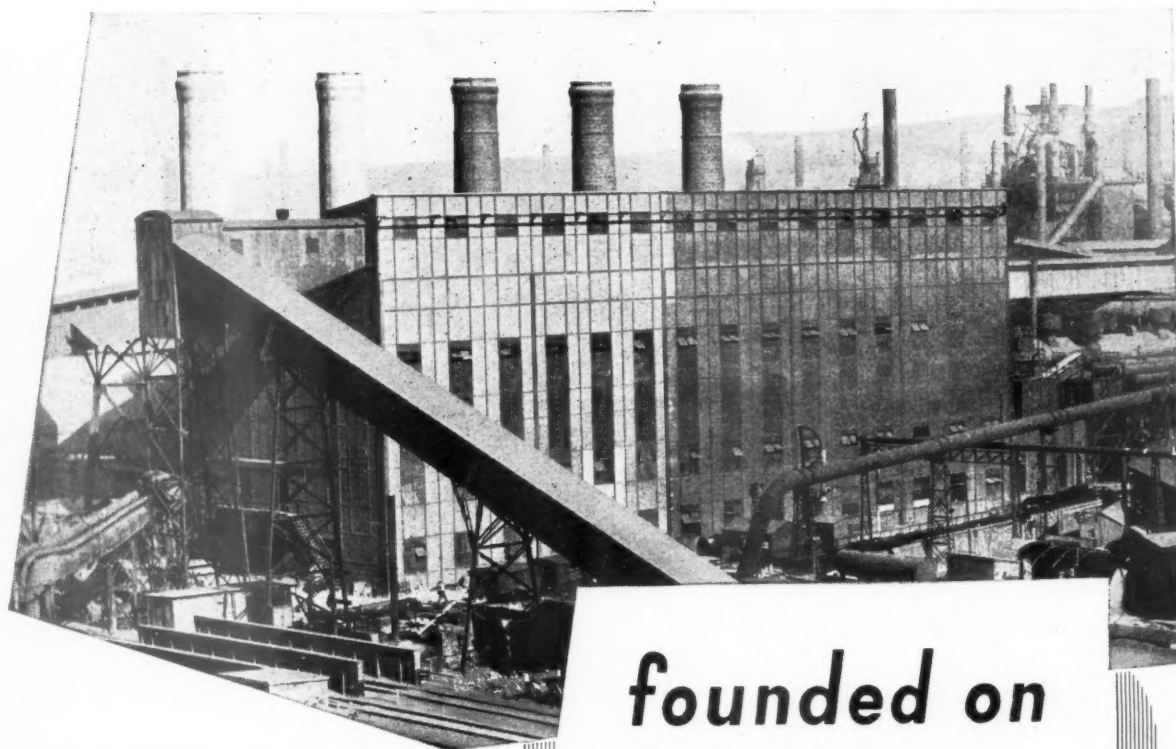
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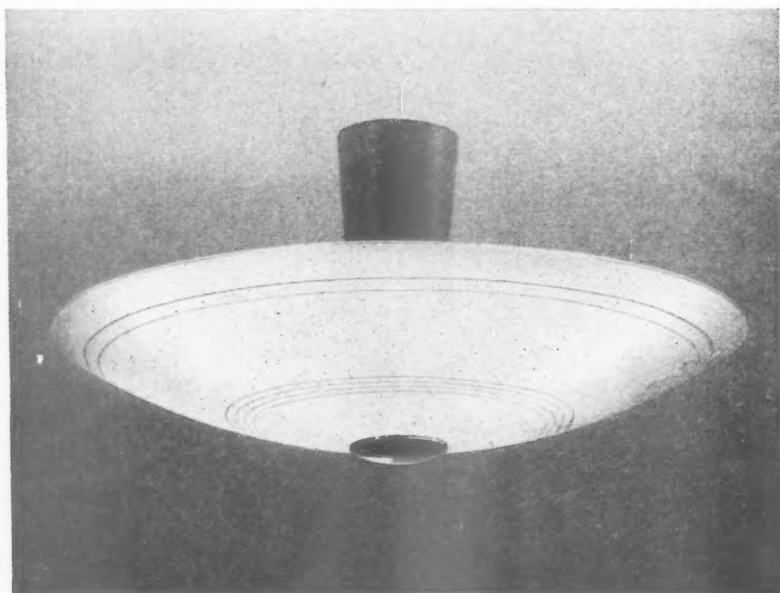
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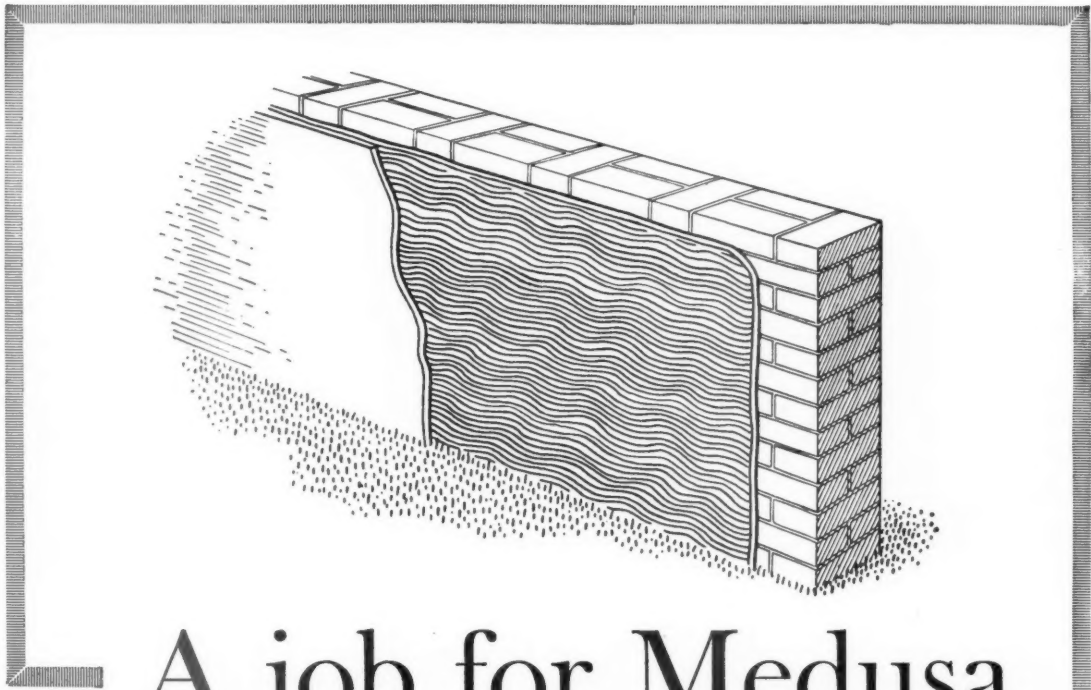
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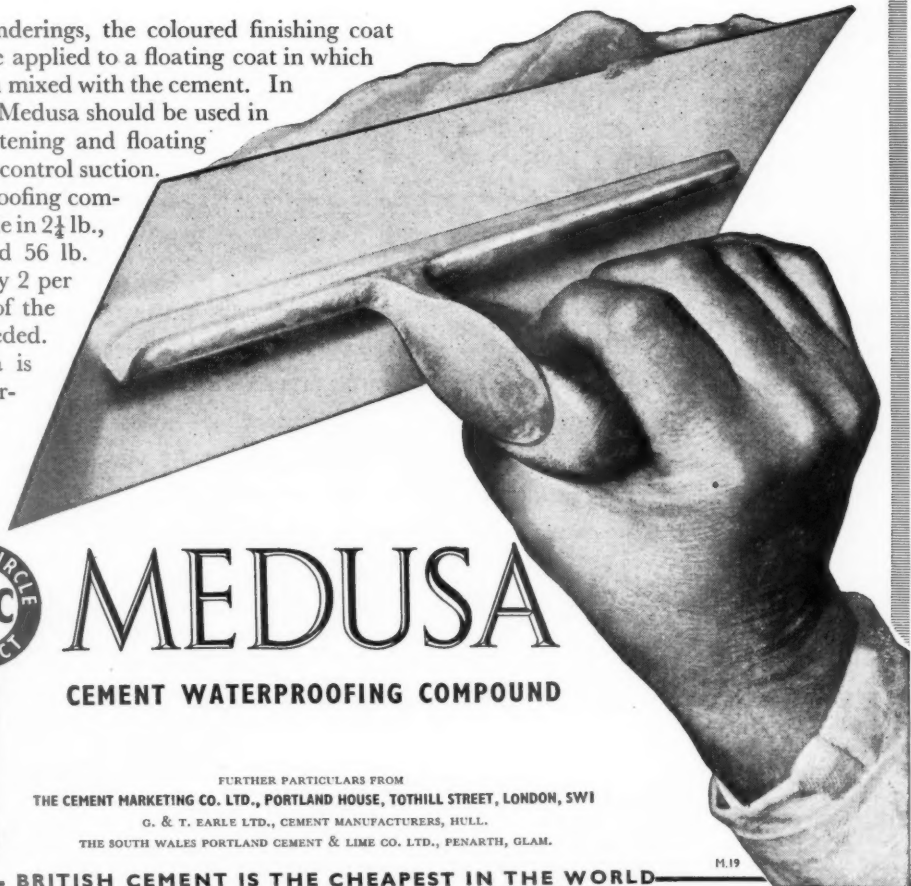




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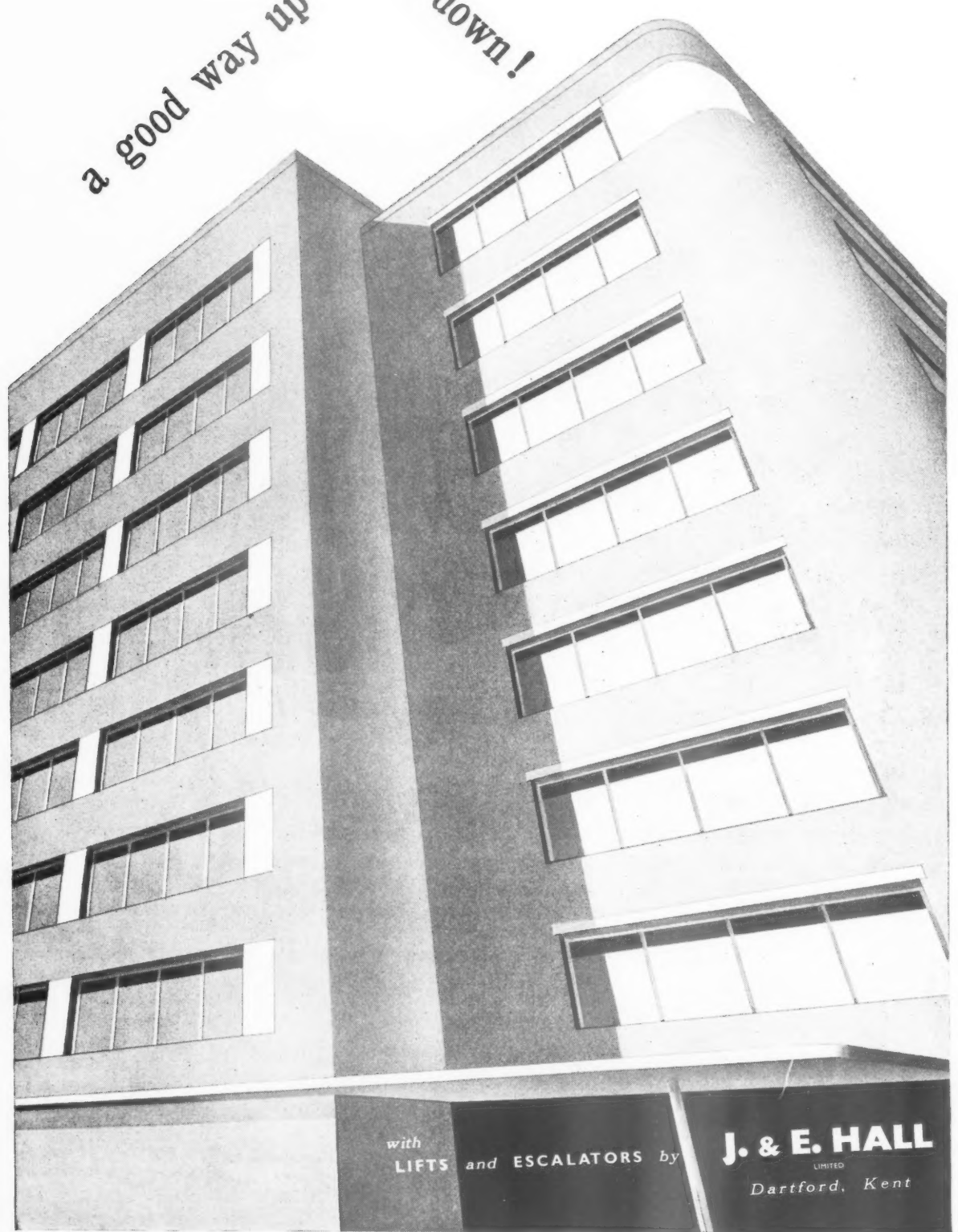
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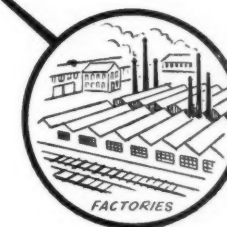
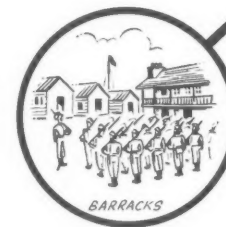
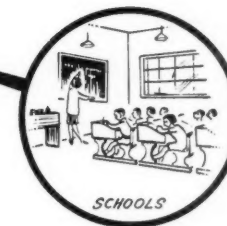
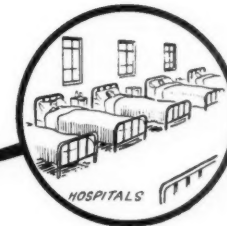
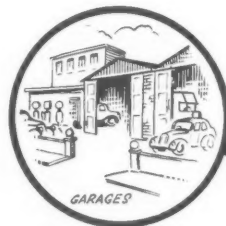
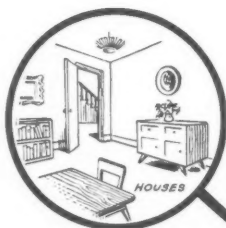


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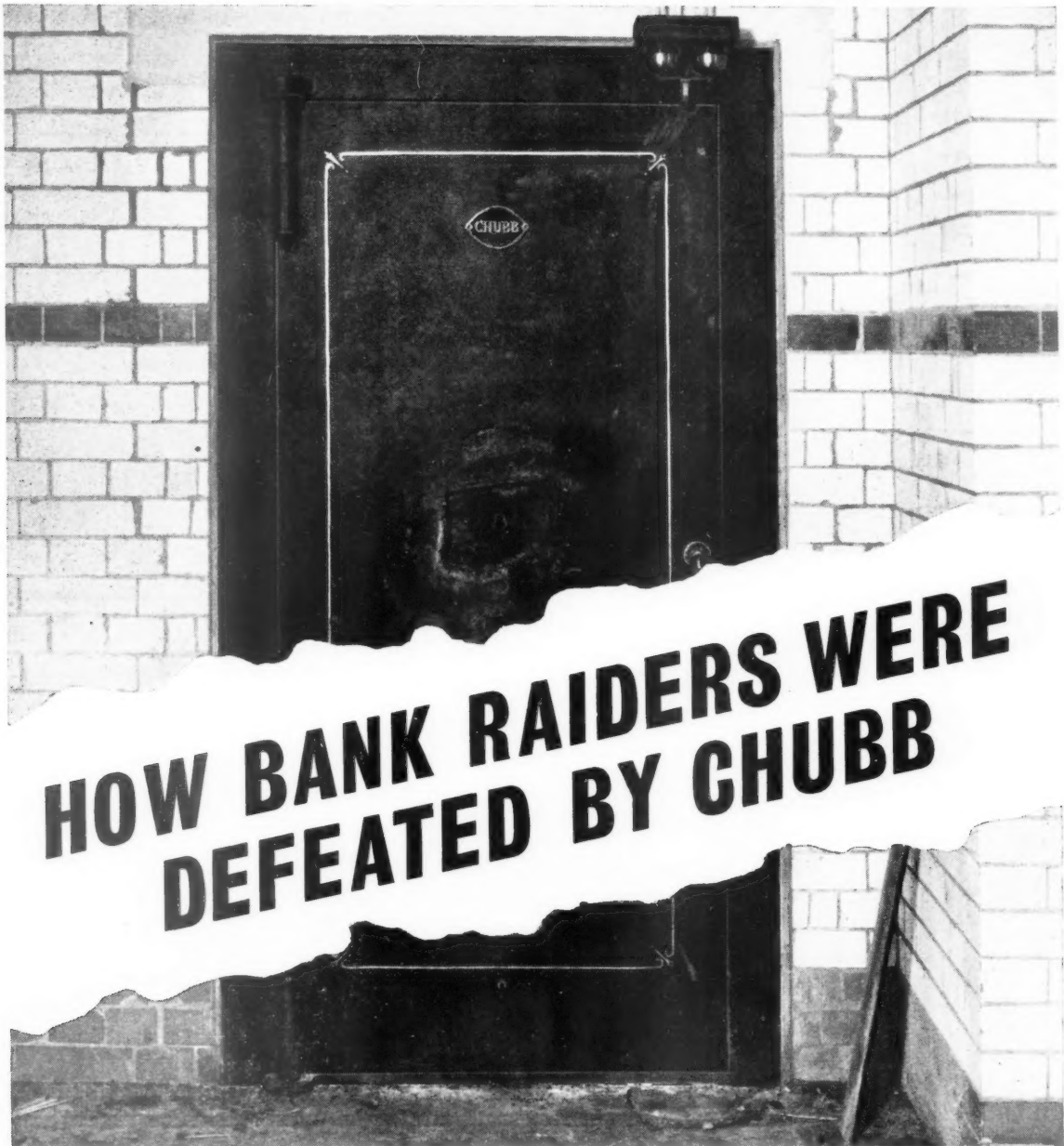
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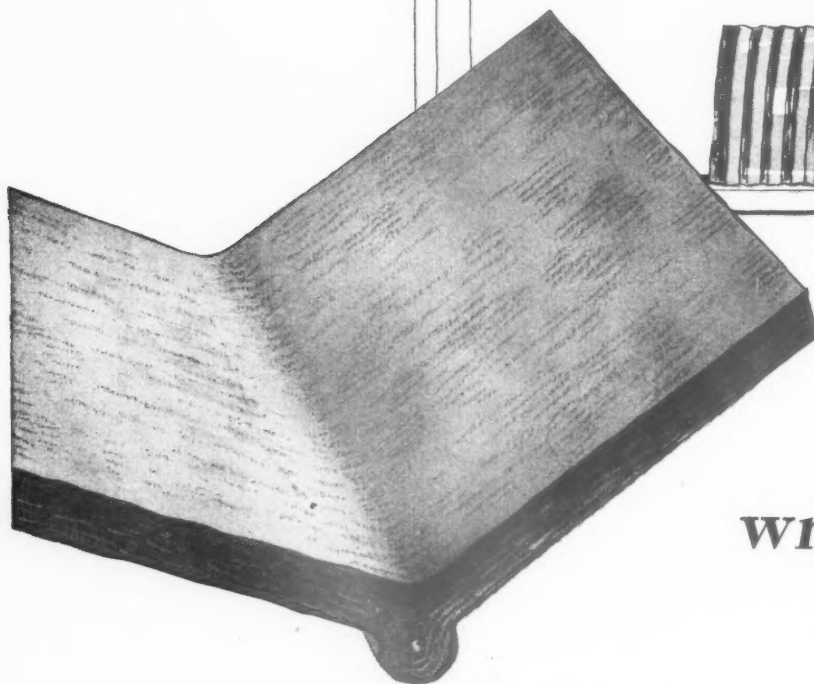
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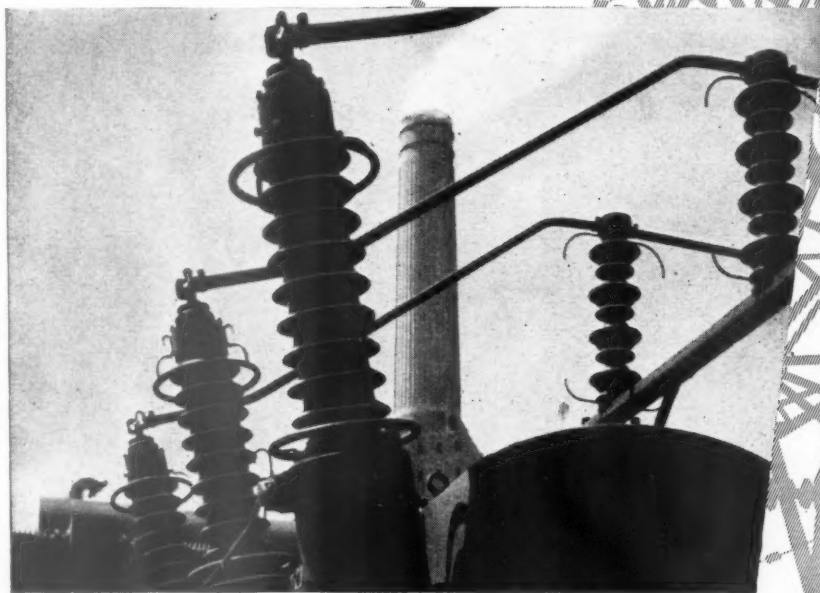
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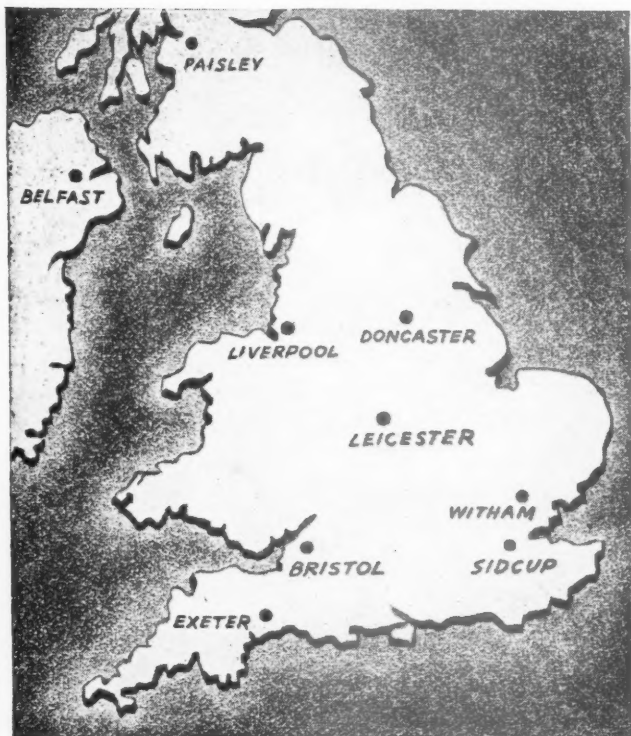
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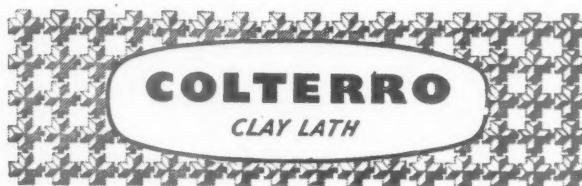
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J. Walter Hanson, L.R.I.B.A.  
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William Wilson, A.R.I.B.A., A.M.T.P.I. (Caithness County Architect)  
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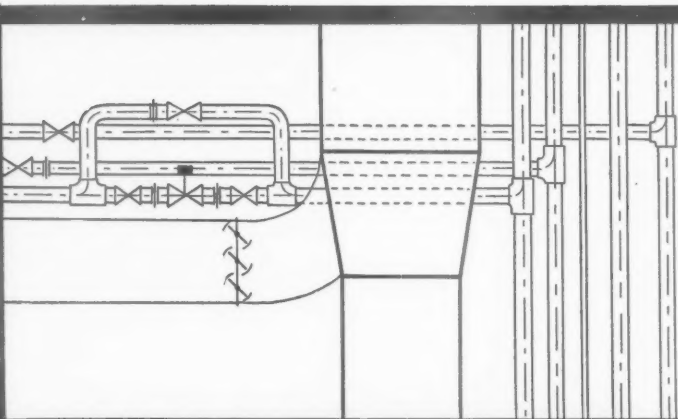
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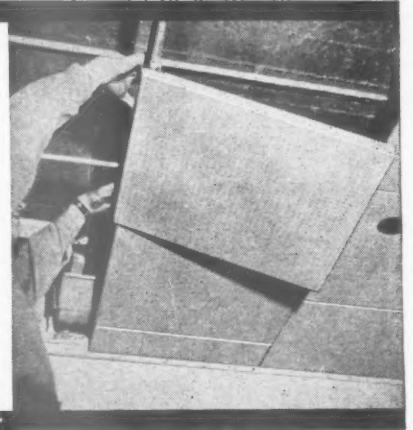
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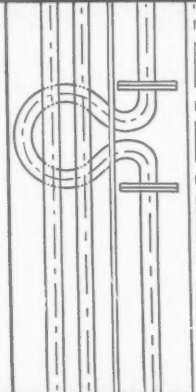
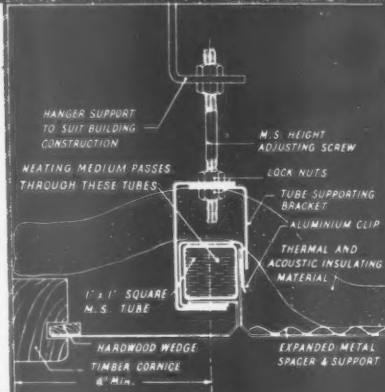
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A Frenger ceiling does three jobs. *First*: it is a radiant panel heating unit with a very quick response to changes in room temperature. A Frenger is made up of a series of square, perforated, aluminium panels suspended from a grid of pipes linked with the hot water system, and overlaid with a blanket of insulating material.



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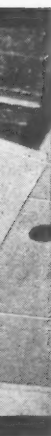
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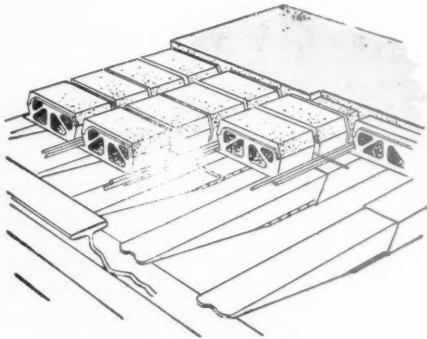
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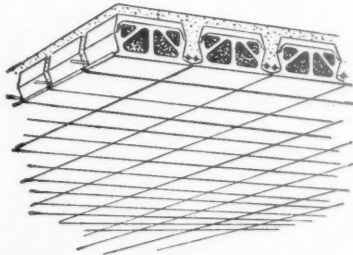
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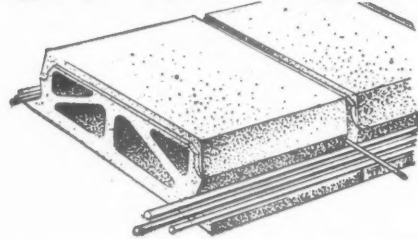
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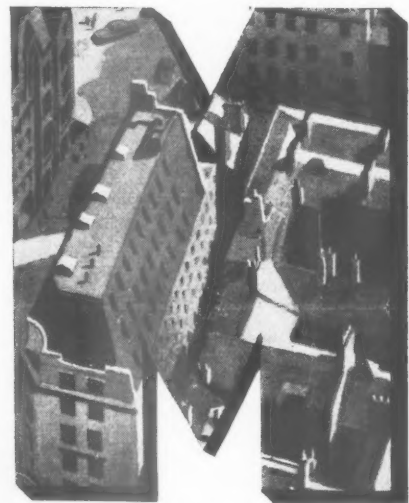
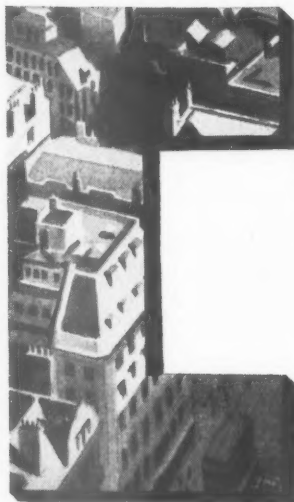
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*Photograph by courtesy of Leeds Corporation.*



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Whether for sound or thermal insulation, or control of acoustics, "Rocksil" provides a most efficient and economical method. It is easily fitted and everlasting—being rot and fungus proof, non-hygroscopic and completely fire-resisting. "Rocksil" has a low specific heat of 0.21 B.Th.U./lb./° F., and a "K" value of 0.25 B.Th.U./Sq.ft./° F/in. thickness at atmospheric temperatures. "Rocksil" can also effect an improvement of impact sound transmission up to 25 phons and for acoustic insulation and modification has a sound absorption coefficient up to 0.96 at middle frequencies. For full details of the uses of "Rocksil" insulating quilts and methods of application write to our Technical Department.

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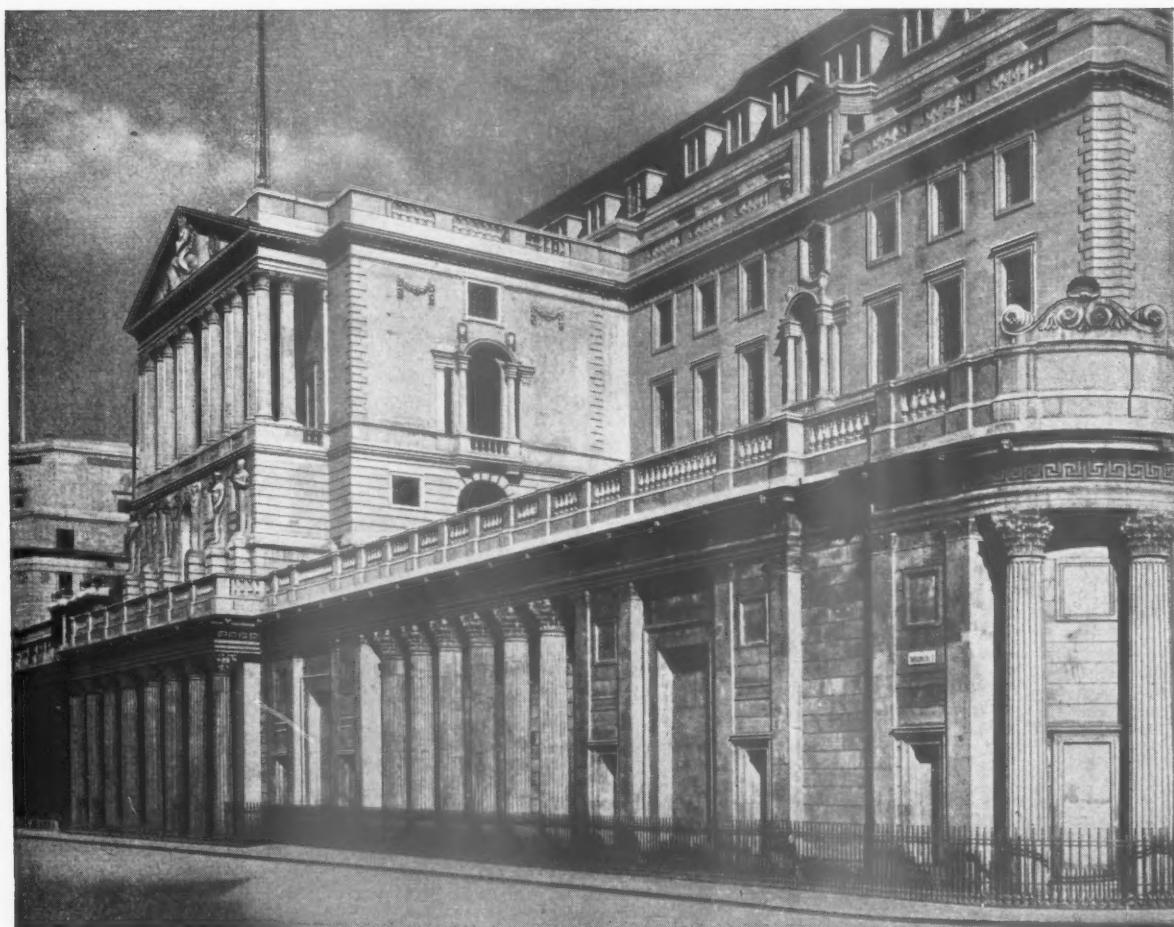
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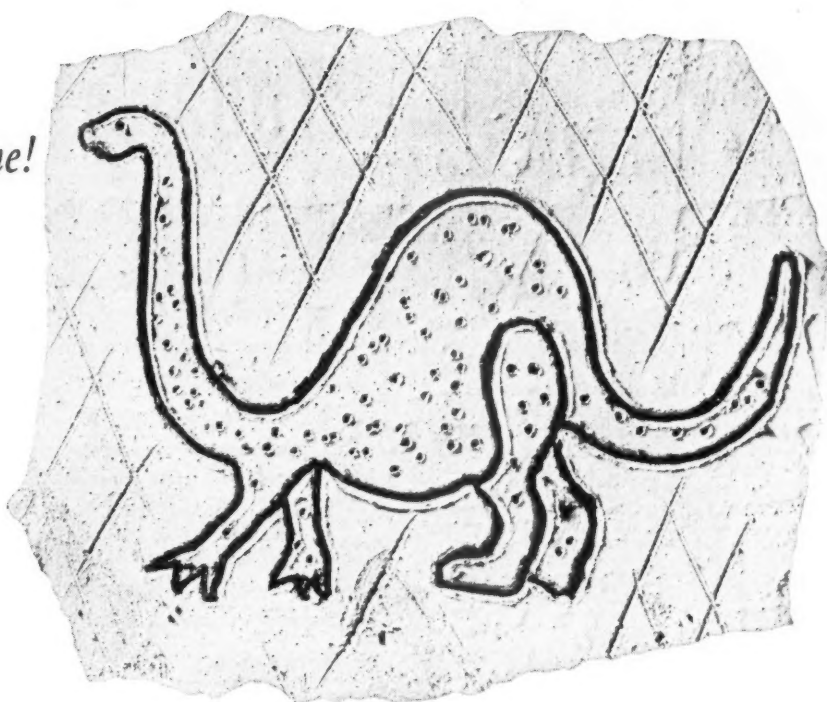
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*"Of course. Then I know my specification cannot fail to be met exactly. Because Carlite is pre-mixed it can be exactly controlled at the factory. Every*

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Extract from "The Sunday Times"

## 'Cut Building Costs' Appeal by Mr. Eccles

MR. DAVID ECCLES, Minister of Works, declaring that the time has come for the building industry to take more active steps to reduce costs and review contracting arrangements, has appealed to the Royal Institute of British Architects to provide a lead in these matters.

In a letter to the Institute he pointed out that, though there had been a marked improvement in the output of building in the past 12 months, there was widespread concern about the level of building costs. There had also been criticism of the contracting methods employed by the industry and apprehension about the existence of restrictive practices.

"These circumstances," said the Minister, "constitute at once a challenge and an opportunity. With the assurance of plenty of work to come, there is every reason for developing contracting arrangements which on the one hand will encourage the proper pre-

# Today's prime need

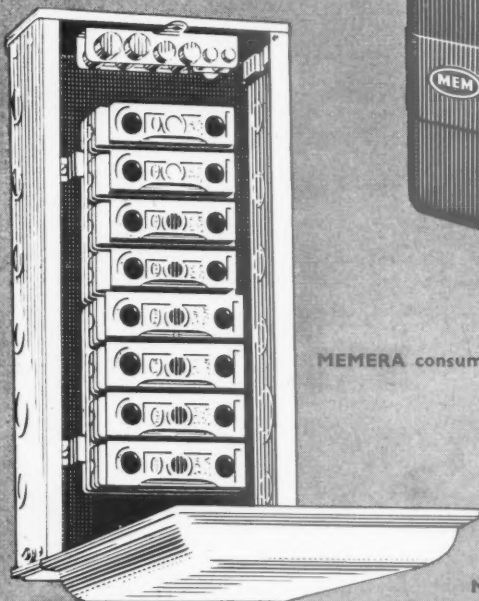
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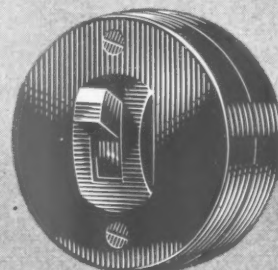
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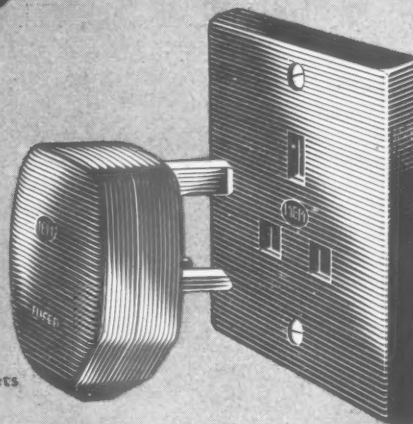
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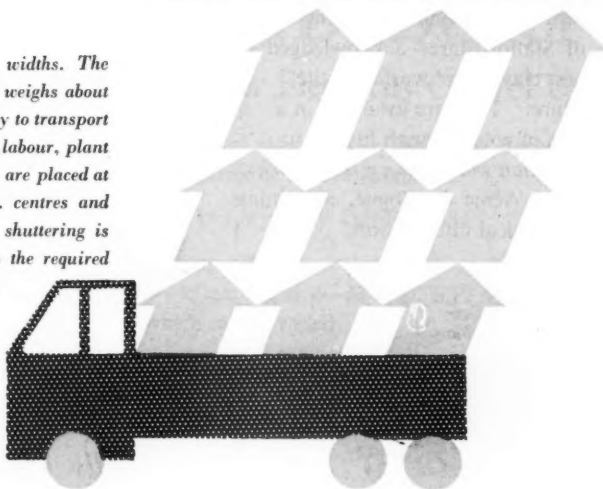
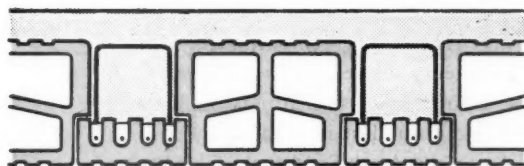
Mr. Howard Robertson, President of the Royal Institute of British Architects, replied: "We shall be very happy to take the lead in this important matter."



Midland Electric Manufacturing Co. Ltd., Tyseley, Birmingham 11  
BRANCHES IN LONDON AND MANCHESTER.

POINTS ABOUT **Stahlton****6****EASE OF ERECTION**

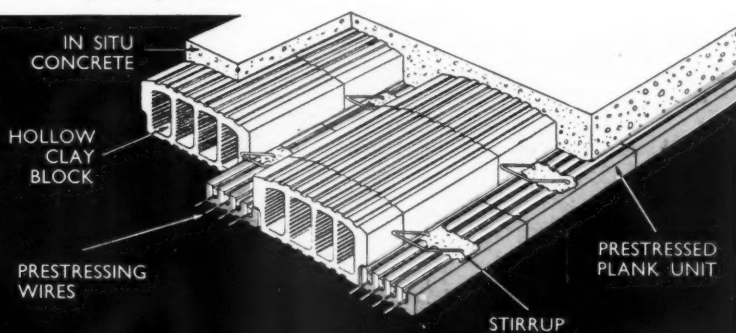
*Stahlton prestressed plank units are made in five widths. The medium width unit measures  $5\frac{3}{8}$  in. wide by  $2\frac{3}{8}$  in. deep and weighs about 12 lbs. per ft. run. Plank and hollow block filler units are easy to transport and handle on the site and contractors can erect with normal labour, plant and under normal site conditions. To erect, the plank units are placed at required spacing with temporary supports at approx. 5 ft. centres and hollow block units are laid end to end between planks. No shuttering is required. The floor or roof is then concreted in situ to the required thickness. Props are removed when in situ concrete has reached an ultimate strength of 2,500 lbs./sq. in.*



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Architect: Leslie B. Bryan, A.R.I.B.A.



*A village street, the old houses cuddling together, their friendly roofs echoing with muted tones the colours of the surrounding acres. The clay tiled roof is part of the picture of Britain—part of a heritage to which we can add.*

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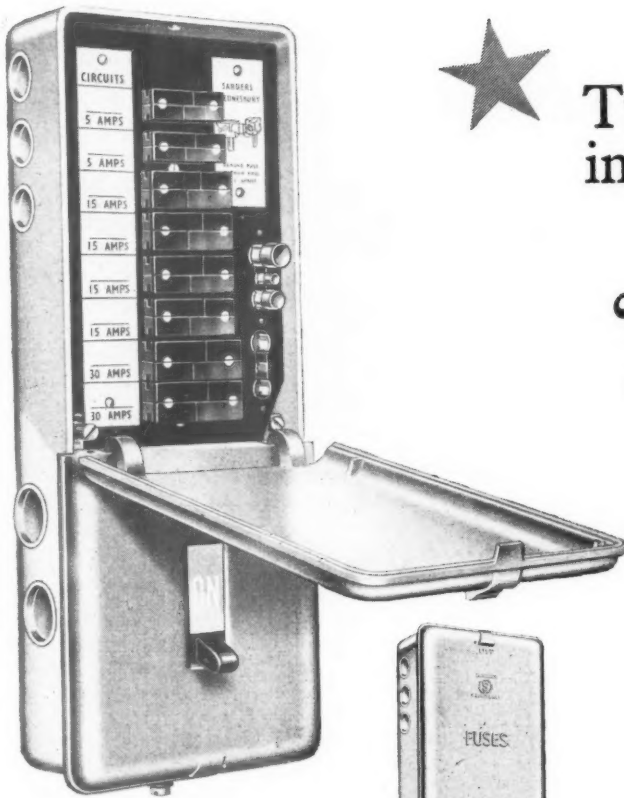
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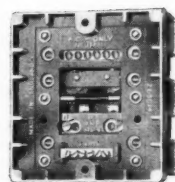
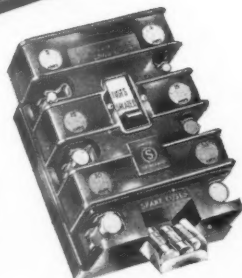
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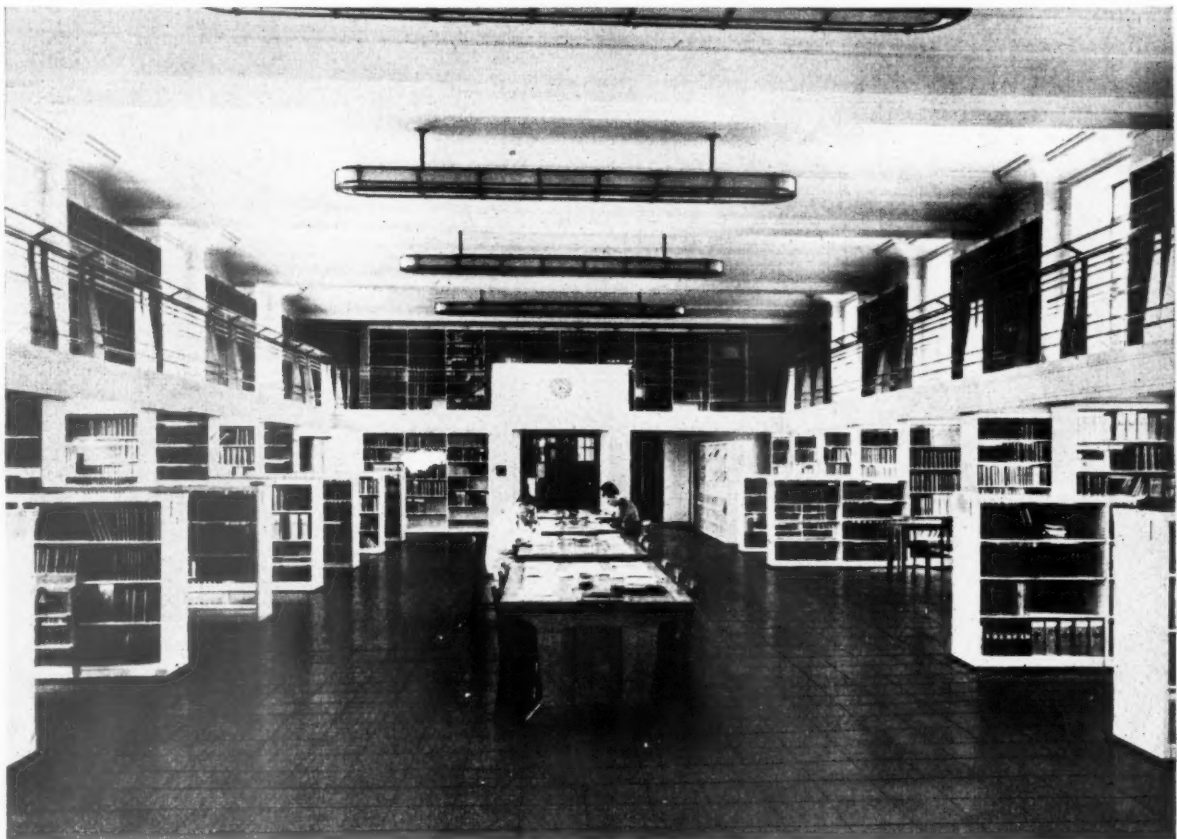


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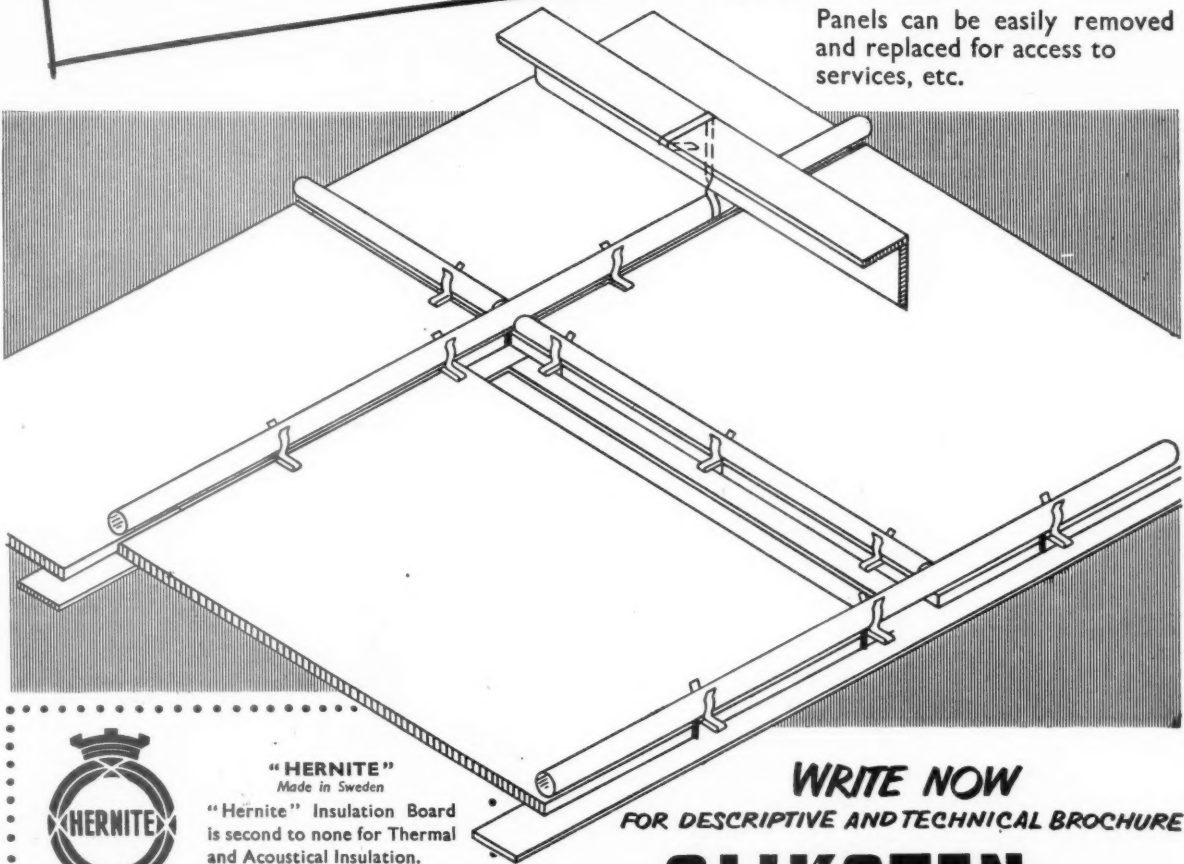
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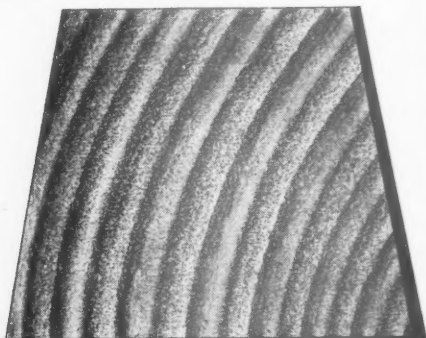
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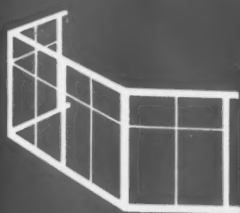
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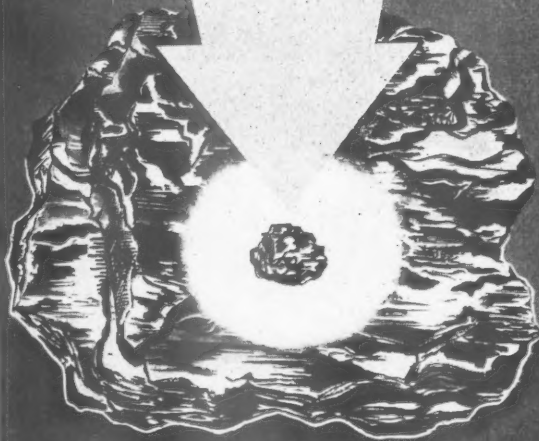
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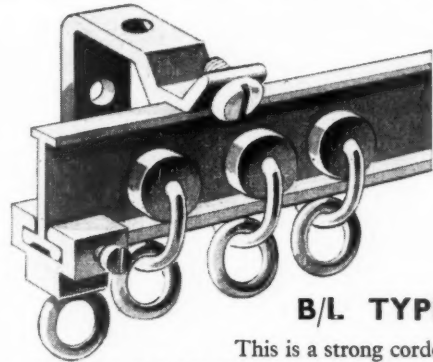


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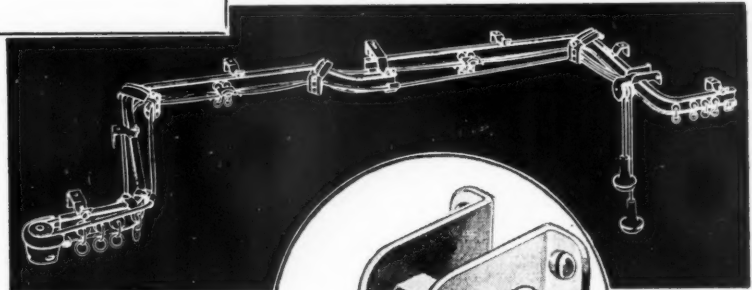


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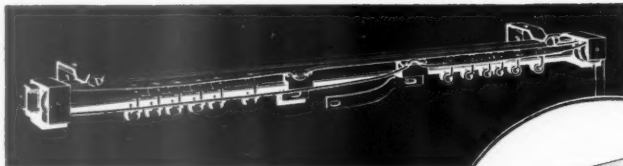
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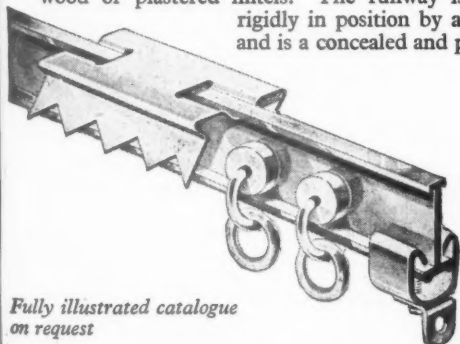
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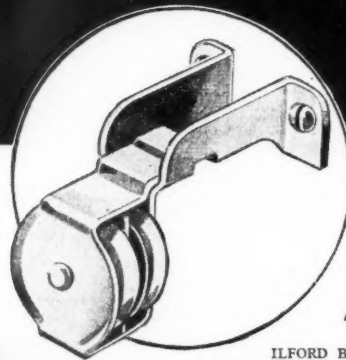
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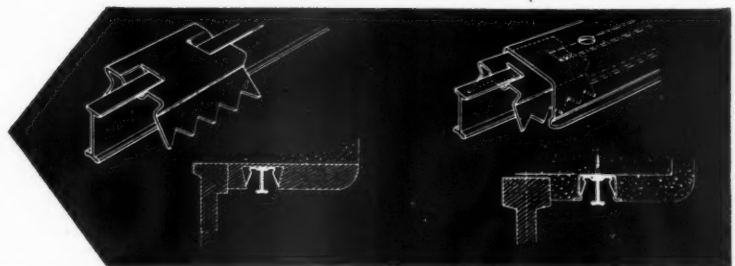


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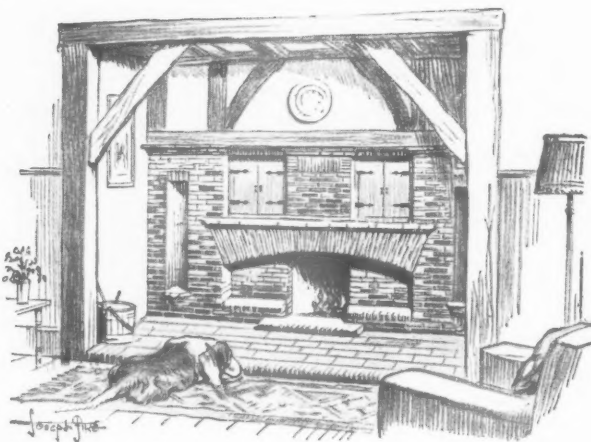
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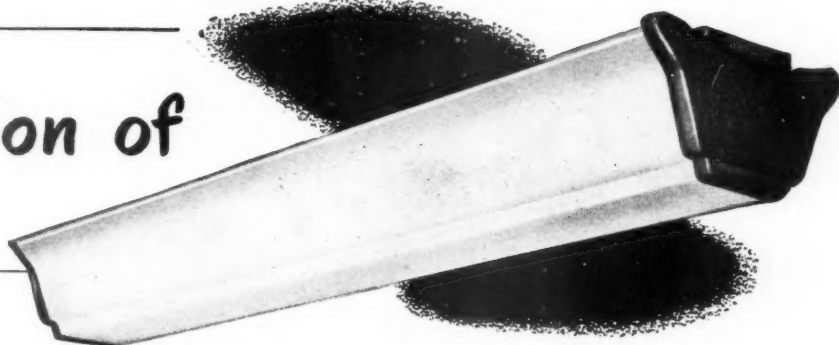
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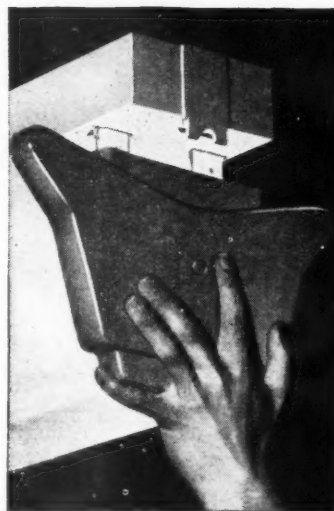


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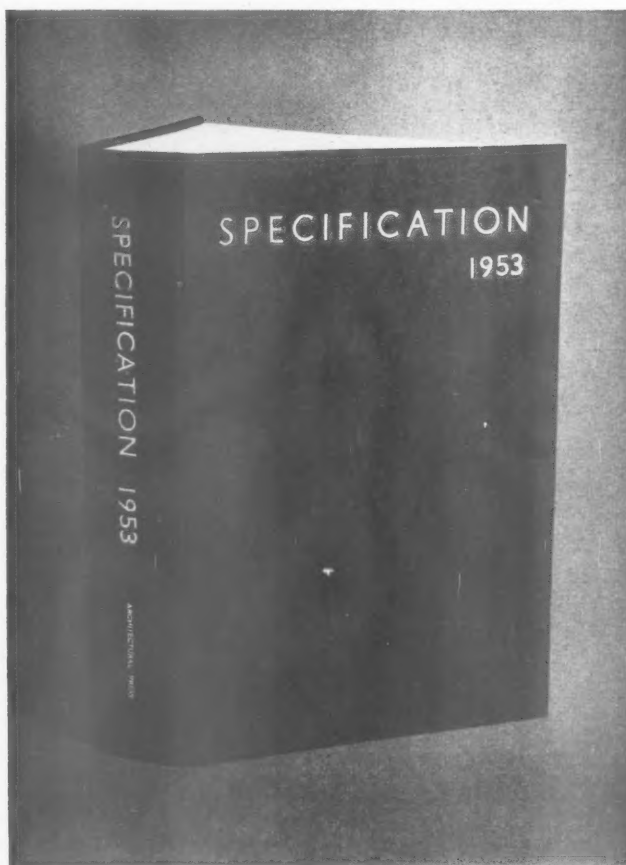
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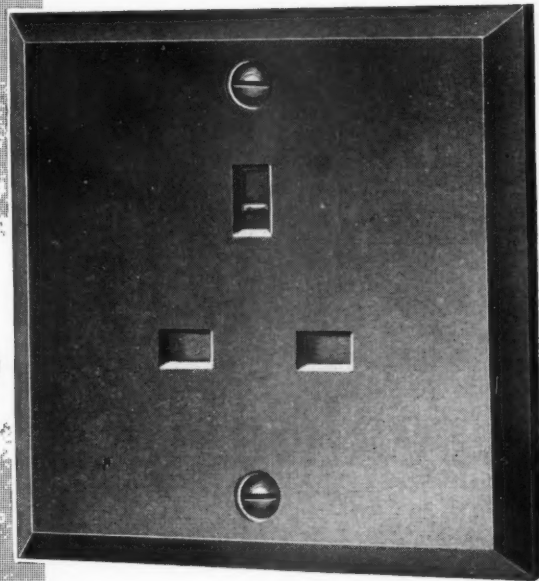
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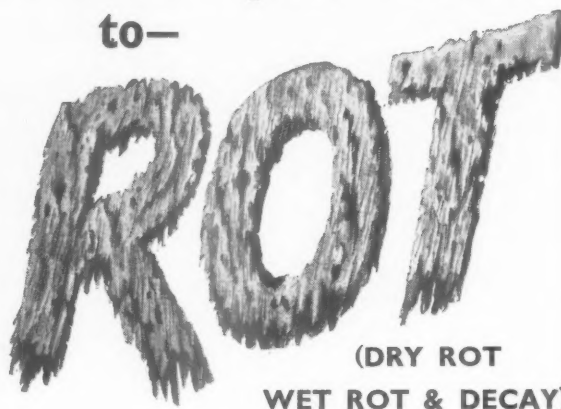
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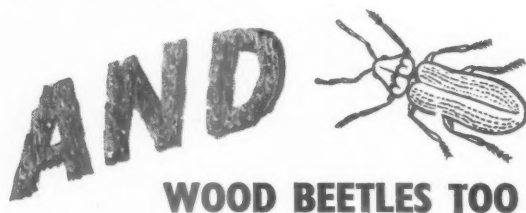
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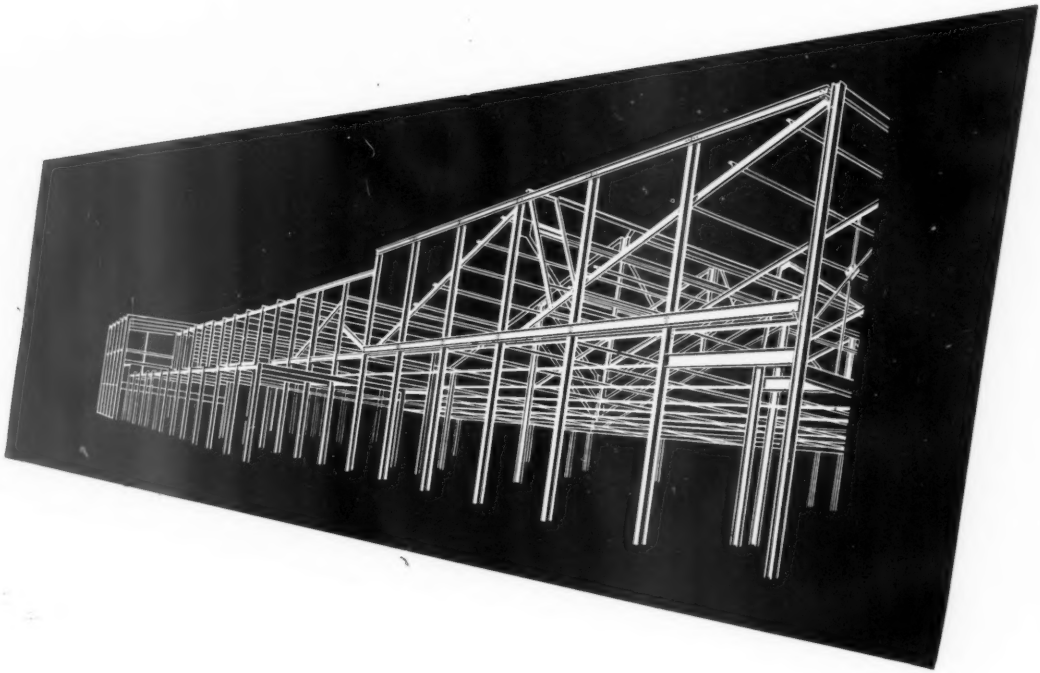
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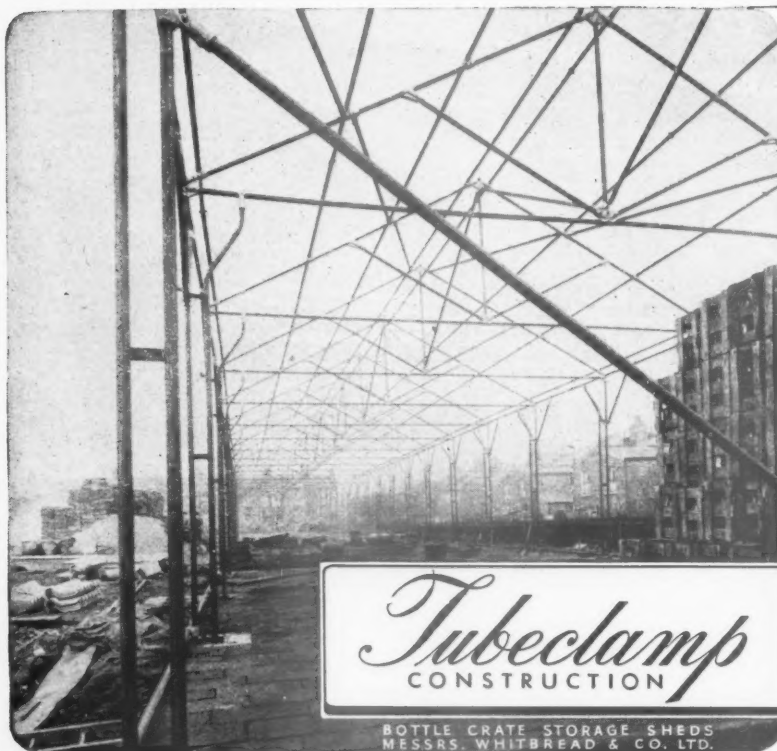
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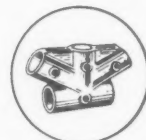
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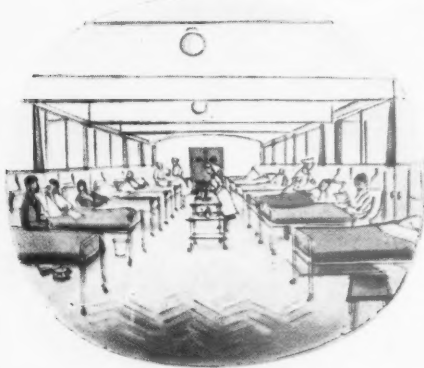
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THE ARCHITECTS' JOURNAL

No. 3054 September 10, 1953 VOL. 118

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## ASHES BY THE SACKCLOTH

For some weeks now—ever since, in fact, he tapped out his rosy-fingered raptures on the new LCC standard house-plans—ASTRAGAL has been guiltily awaiting the obvious retort . . . that good and ingenious as these plans undoubtedly are, so they jolly well ought to be if you can get away with an inside bathroom and w.c. This vital difference between MOHLG and LCC plans was never referred to on these pages, I'm afraid, nor in the even more rapturous leading article of the same issue (August 13). Granted that it may be more sensible and economical—though never so pleasant—to have a windowless bathroom (remember Betjeman's poem about the toothbrushes airing on the

window cills of North Oxford?) ASTRAGAL feels, in retrospect, that it would have been fairer not to rebuke MOHLG so sharply (if indirectly) for not doing something which they had clearly not set out to do. And, giving a final hitch to his sackcloth (with its cuffed sleeves and flapped ticket-pocket), he confesses to a second error—the most infectious of all critics' diseases—which is that of assuming that, because one thing is good, something else is necessarily bad. It is possible, and quite often occurs, that both are good. Why hasn't some correspondent written to this effect, and thus saved this hawking around of ASTRAGAL'S conscience? Luckily it's the tail end of the Silly Season.

\*

The Silly Season—which to an architectural paper is a season too trivial and slap-happy for the serious discussion of architecture—is for the non-technical Press (and for exactly the same reasons) just the time to fill up the pages with a few bits about architecture, witness . . .

## STAMPED ARTICLE

. . . the *Daily Telegraph's* recent pieces about how to be, and how to choose, an architect, which has now been followed by an article by Reginald Stamp on the planning of London's housing development. Mr. Stamp speaks with authority on this subject—he should, he is chairman of the LCC housing committee, but for this reason he has to speak in defence of an established policy of decanting Londoners all over the home counties, rather than give a radical analysis of the problem and its possible solutions. As a defence of a planning procedure which has now become a routine rather than a great

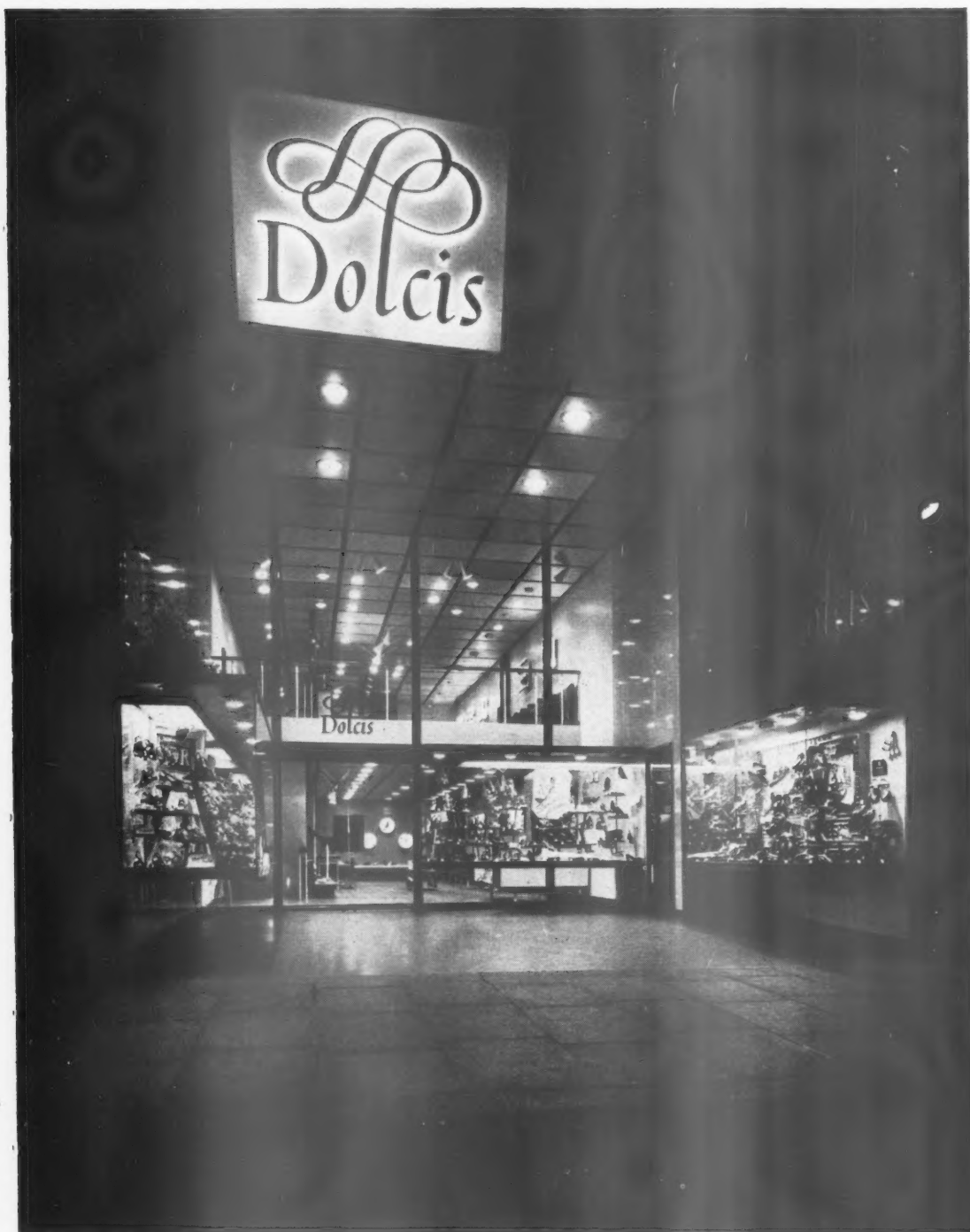
human adventure this article is fair enough, but the constant use of terms like "decant" and "overspill," as if the human race were an incompressible liquid which could be piped about the country at will, eventually becomes rather creepy-making, if only because it shows how habitual and inhuman our thinking on town planning tends to become. ASTRAGAL has not the slightest doubt that in his human and fallible way he will relapse into using both words again before long, but at least he will wince when he sees them in print over his own signature.

## CENTRAL PARK, NEW YORK

New York's recent celebration of the centenary of the inauguration of Central Park reminds ASTRAGAL how much this "most expensive plot of grass in the world" owes to Sir Joseph Paxton. For it was Paxton's almost forgotten Birkenhead Park which influenced F. L. Olmsted in his design for Central Park. Opened in 1847, Birkenhead was the first park specifically acquired and designed as a public park. It was intended as an amenity attraction for William Laird's rival new town to Liverpool. Even if little came of Birkenhead, the park was a great success, and was a source of pilgrimage from visiting foreigners, especially Americans coming through the Port of Liverpool.

\*

Olmsted visited Birkenhead in 1852, five years before he and Calvert Vaux won the Central Park Competition. With its paved carriage ways separated from pedestrian ways, boating pools, bandstands and playing greens, Central Park adopted the Birkenhead pattern.



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In its turn, with its slow and fast traffic routes, under and over passes interspersed among trees and lawns against a background of skyscrapers, it has been the model for the green cities of Corbusier and his followers—even though some New Yorkers will tell you that they would not walk through it alone after dark, for fear of hoodlums. But you can't blame Olmsted or Corb. for *that*.

#### LETCHEWORTH

And while in the anniversary belt, a very warm—if belated—note of congratulation to our miniature green city of Letchworth, celebrating this year its jubilee. Many of the ideas for which Letchworth stands have been abandoned, or are today facing criticism, but with all its faults it is still the first example. A brave pioneer, still a place of pilgrimage for town-planners from all over the world, and a popular home for the residents—a high proportion of whom can get home to lunch, as Ebenezer Howard always hoped they would.

#### TWO CHEERS FOR EMBROIDERY

Some of you may remember David Langdon's drawing of a man leaping excitedly into a taxi and shouting to the driver, "Royal School of Needlework and drive like Hell." There's no need to get quite so excited about the current show at Artists' House, Manette Street (just behind Foyles in Charing Cross Road), where a group of young embroideresses from Bromley Art School are displaying a range of their work, but some of it is very elegant, and its potentialities for interior decoration are, as the artists claim, considerable, and as yet almost untested.

\*

It was interesting to note that the machine embroidery to be seen at this little show in London was far richer and more decorative than the hand-work. This should encourage those who would like to have a shot, but don't visualise themselves stitching away the hours in an upper window while waiting for Sir Lancelot or some other rescuing knight to come along and give them a commission.

\*

Only two cheers, then, for embroidery at present. The third one—like E. M.



*The north-east elevation of Bucklersbury House, which has been revised to suit the clients' wishes—with suggestions made by Sir Giles Gilbert Scott. This elevation was prepared at a later date than the model illustrated on page 316. The revisions were put before the RFAC yesterday.*

Forster's tribute to democracy—must be, for a time, withheld.

his work find it good: one may come to scoff—but usually stays to praise greatly.

\*

#### PHILIP JOHNSON

Among returning American visitors passing through London, ASTRAGAL was delighted to catch a brief glimpse of Philip Johnson. Best—if not only—known here for the fabulous glass house he built for himself in Connecticut, in the USA, Philip Johnson is a figure of considerable controversy. As director of the Department of Architecture and Design of the Museum of Modern Art, he has long been largely responsible for the Museum's excellent architectural exhibitions and publications and, being a man of very decided opinions, has received both wild praise and bitter criticism. But it is as an architect that he would claim our attention. His practice has been, to say the least, selective, consisting of a number of unusually fine, though not large, houses—two of them for himself, and other for clients whose names are household words in many lands. He has recently added a wing to the Museum building, and redesigned its new sculpture garden, an elegant design requiring a minor Le Notre operation in earth moving. There are those who see him as merely a disciple of Mies van der Rohe (whose biographer he is) and others to whom he is "the arch classicist." Yet most who know

As he is an admirer of the works of Robert Adam and Soane, it is not surprising that some of his later work shows a preoccupation with domed interiors. You may be interested to know that in his brief visit to England the two buildings he saw and enthused about most were R. Adam's Osterley and the Smithson's new school at Hunstanton (discussed in this issue).

#### EXHIBITION SEASON AGAIN

ASTRAGAL went to two dim exhibition previews last week—the Radio show at Earls Court and the Marine show at Olympia; dim, he hastens to say, only



literally. The first show has—at last—an officially appointed architect, but it looks as though he didn't have much money to spend, and the individual stands are as uncontrollably strident





### *Preservation Before Reconstruction*

The war-damaged church of St. John the Evangelist, in Westminster—described by John Summerson as “a building of the highest importance in the history of our architecture”—is to be rebuilt as an archive house for the Diocese of London. But the necessary repairs to this church, which was built under Queen Anne’s commission (1721-28) and reconstructed inside after fire in 1758, cannot be carried out—it is said—for another fifteen years.

In fifteen years, London’s weather can do a lot of damage to the church’s shell if it is not protected. We hope that the diocesan surveyor (the church belongs to the London Diocesan Fund) will undertake periodic surveys to ensure that the structure is kept in good repair. And we are prompted to ask a question: Are there other buildings, of historic importance, which are similarly in need of preservation while awaiting delayed reconstruction?



## POINTS FROM THIS ISSUE

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as ever. As for the exhibits, you'll be pleased (or will you?) to know that attempts are being made to provide you with TV sets that you can match



up to your furniture—whether Queen Anne or contemporary. (See my illustrations, which include a set that fits snugly into a corner.)



ASTRAGAL couldn't help noticing how much better are the designs for marine radar than those for TV sets. Is this just another example of the



traditional nautical flair for design, or is it just that marine engineers don't have to try quite so hard to catch the consumer's eye?

ASTRAGAL

## The Editors

## WATCHDOG ON THE PROWL

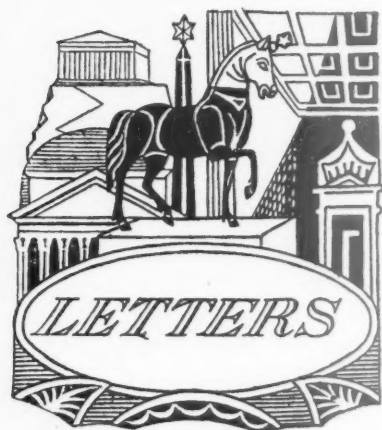
THIS summer has been marked by an unusually vociferous barking from the watchdogs of our national heritage in both town and country. (You will remember—among other things—the baying which has helped to prompt an enquiry into the proposed television mast on west-country moors, so that viewers may have their cake and [between programmes] eat it *al fresco* unaffronted). It is encouraging to hear that one watchdog is to do something really constructive. The Council for the Preservation of Rural England proposes to list “outrages committed in the name of public service” against the countryside.

Equally as pleasant as the news that the Council means business is the wit with which it announces its proposals. The five suggested headings for the black list are “structures ; dumps and disorders ; excrescences ; natural and unnatural objects ; and miscellaneous.” There is no doubt that ridicule is a more effective spur than the most deep-throated uproar. We look forward with interest to seeing the dossier to be compiled by the county branches of the CPRE. It should be a lively document.

## THE JOURNAL'S GLOSSARY

Since we published our glossary of building terms on August 20, with the intention of helping readers to avoid confusion in discussions on modular co-ordination, several readers have written to remind us of the MOE's definition of the word “element” (see letter on page 310), which differs from our own definition.

It is not true to say, however, that the MOE's definition “is already accepted usage.” We would ask our correspondents (and other readers) to be patient and to give our glossary a trial. We think they will be surprised to find how easy it is to use the three terms—building material, building element and building component—as we have defined them, and how quickly their use in this way becomes automatic.



R. A. Sefton Jenkins, B.Sc.,  
A.M.I.C.E., A.M.I.Struct.E.

"Registered Architect"

Richard H. Davies, F.R.I.B.A.

David M. R. Keate

## Modular Co-ordination: AJ Glossary Criticised

SIR,—I hesitate before criticising such an excellent authority as Bruce Martin and the glossary of building terms which you say he has helped you prepare (August 20 and 27), but I must say that I feel that in defining a building element you have brought about more confusion than the architect whom you state "brings confusion to discussions about modular co-ordination" by thinking of a component and an element as synonymous.

As you must surely know, the Ministry of Education has issued a series of Building Bulletins dealing with schools. In this the word element is used repeatedly and means, say, a roof complete with the necessary waterproofing, but not including the frame, which is another element. This, you will notice, fits your dictionary definition of a "component part" (of the building) and an "essential part." It is, also, in an elementary way, a "first principle."

I would prefer to see bricks and other common materials defined as "materials," and "components" as the parts that go to make up an "element."

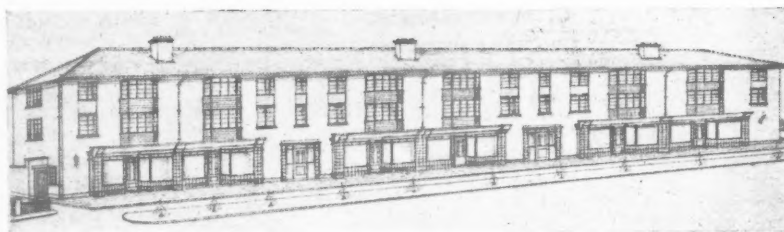
Even the dictionary makes the confusion worse as it states that a component forms "one of the elements of a compound."

I feel that it matters little what a thing is called provided that everyone else will understand what is meant when a word is used. The Ministry of Education has been using the word element to mean a completed portion of a building since 1951. I see no justification whatsoever for defining an element as something different from that which is already accepted usage.

R. A. SEFTON JENKINS.

London.

[See editorial reply on page 309.—Ed.]



This sketch of shops to be erected by Warwick Corporation is the subject of two letters published below.

## Shops for Warwick

SIR,—In the JOURNAL for August 27 you publish a sketch of the first block of shops to be erected by Warwick Corporation, designed in the borough surveyor's office and submitted by a Warwick reader who appeals for ASTRAGAL's support in his criticism that it is not a good design.

I gather from ASTRAGAL's remarks that he does not think much of the design. I disagree with him.

I form my opinion not on the sketch, which could have been better prepared and would have given a different aspect of the design, but by comparison with the many illustrations of similar buildings that have appeared in your JOURNAL.

I consider that ASTRAGAL's action in illustrating the sketch, asking what readers think conveys a prejudicial feeling towards a borough surveyor's office, where possibly a fully qualified architect is employed.

"REGISTERED ARCHITECT."

Anglesey.

SIR,—While one has no hesitation in supporting the criticism of the Warwickshire reader that the block of shops you illustrate is not a good design, surely if Warwick has, as you say, a population of as little as 15,000, would it not be preferable that it should form a panel of local architects, rather than set up an architect's department to cope with its official building programme?

RICHARD H. DAVIES.

London.

## Plymouth

SIR,—The difference of opinion between myself and ASTRAGAL is more apparent than real. This is probably because my belated reply\* to your excellent article of June 12, 1952, was occasioned by less sound criticism.

I sought in my unsolicited defence of J. Paton Watson to stress that any defects in Plymouth's post-war buildings cannot necessarily be blamed on the plan of which they are a part. To do this is to fall into the error of arguing *post hoc, ergo propter hoc*. I advisedly stressed that in Plymouth the engineer must control the whole, because J. Paton Watson's title and qualifications are those of "engineer," and he is the city's planning officer. I could happily have written "the architect and architect-planner must always work together, but it is the latter who must control the whole." The responsibility for that which disappoints lies with developers and the architects they have employed.

The real tragedy of Plymouth is that any lack of standards in the co-ordinated centre is unnecessary. The pages of your journal illustrate what the best of contemporary architects can produce in this country. But

their lesser brethren are not unjustly criticised by Mr. Marples. They seek to make excuses, whereas your journal can afford to welcome indulgently the remarks of a layman such as myself.

So far, in the realization of "A Plan for Plymouth," more money than imagination has been used, but there is still time for the balance to be redressed. Otherwise our Plymouth may indeed become a ponderous monument, and its reconstruction not only the city's greatest triumph but its biggest disappointment.

DAVID M. R. KEATE.

Plymouth.

*Lance Wright has written us the following news letter, covering events in Bristol during the last few months.*

## BRISTOL News Letter

BRISTOL has been more slow than any other great city in building up her flattened centre. It would be pleasant to think that this slowness of hers is the result of some inner dissatisfaction with current formulæ: that she was watching the fatal, costly decisions of her more eager sisters—Plymouth, for instance—with the object of profiting by their mistakes.

But it seems more likely that her endurance of desert wastes results simply from her decision to give first priority to housing—to "decanting the over-spill into the outskirts" as someone at a Planning Conference has described it. For this long prostration of business facilities must be too costly, in terms of goodwill, for any Corporation willingly to prolong it. Also it must be admitted that the one fragment of the core which has so far been rebuilt—Broadmead—has not provoked the criticism which it deserves. It seems that people are so charmed at getting any shops at all that they feel it ungracious to question the manner in which these shops are disposed. Yet it is difficult to see any point in the acquisition by a local authority of expensive sites if these are to be developed on

\* Published in *Western Morning News*, August 10. See ASTRAGAL, August 20.

principles which are no more enlightened than those which applied before. All that can be said in favour of the new Broadmead development is that it has secured more regularly shaped blocks, on the pattern of the old Lex Adickes of imperial Berlin.

### "CLEAN-SWEEPISM"

This desire to "square up" and its concomitant, "clean-sweepism"—a restiveness against such older buildings of value as seem to stand in its way—is historically an early, primitive mode of asserting the Public Interest in Town Planning. Its vices are carelessness of the human scale, scepticism of the value of informal three-dimensional spaces piercing the corridor street and a hearty disbelief in the "time dimension." Town Planning Theory changes fast. Those who have lost faith in Man's ability to create a living order say that the Planners don't know their own minds. To outsiders this may well seem to be the case. But to those who feel themselves to be "engaged" in the City, these apparent about-turns are only successive facets of a single developing theory: they happen because of the progressive enrichment in our Idea of what a City should be.

This "developing doctrine" is difficult to explain to City Councillors who arrive jubilant at one stage in the unravelling of the mystery and are then shaken and annoyed to find that the professional thinkers have carried the matter several stages further and have put quite a new complexion on much of what was held for sure.

This is particularly the case in Bristol where the Corporation has long been instructed by its able first Planning Officer, H. M. Webb. Mr. Webb has now retired—bequeathing a Plan. His services to Bristol have been great indeed. He and his admirable former assistant, Mr. Brueton, have given to their Masters an excellent schooling (though they would not put it this way) in Primary Planning Education. But "it is in flowing down to the sea that the river is true to its source." The difficulty now is to make Bristolians realize that the most handsome tribute which they can pay to the high principles and shrewd commonsense of Mr. Webb is to think again about his Plan.

In the end, the root difficulty of all English City Planning is that Englishmen have never recovered the "Idea of the City"—regarded either as a built-up Thing or as a way of life. The shock of industrial disorder was so great that, when they came to ask themselves "what is to be done?" the only answer they could find was to do away with the city altogether: to encourage everyone to live outside and to make the centre so full of light and air that it is not recognizably a city any longer. It has become so diffused, so thinly spread over so wide an area that it vivi-

fies no more as a thought. For it seems that exceptional concentration and a well built-up core is not merely a thing to be desired for practical ends but is an emotional necessity.

Bristol affords a striking example of what happens, in terms of a people's outlook, from this failure to build up the hub to the measure of the times. For though she evokes an exaggerated civic pride, she evokes no civic sense: no sense, that is, of how to live in a city, of the compensations—to set them at their lowest—of city life as opposed to life in the country. Thus she has far less than her share of such facilities as Eating Houses, Restaurants, Hotels and places of Entertainment—places which are, after all, a fair index of whether a city life is being lived or not.

In this matter it is worth noting that such evidence as has come down from past times makes it quite clear that this has not always been so. It would be pleasant to think, therefore, that the pendulum has swung to the extreme limits of unsociability and is now on the point of swinging back. But it is hardly possible for the poor planner to hurry the swing unaided: for he can hardly plan for social habits which do not as yet exist: he needs at the very least a climate of opinion to back him.

Nevertheless—on so many scores—this is a fateful moment in the history of this provincial capital.

### LUBETKIN BUS SHELTER

The village of Hillesley, near Wotton-under-Edge in the Cotswolds, has recently got a bus shelter by Berthold Lubetkin, who lives there. It seems that Mr. Lubetkin has volunteered his services to the Parish Council, who had settled on a bus shelter as being the most enduring means of celebrating the late Festival of Britain.

What followed is not easy to unravel and—for those who were conscious during the early 'thirties—difficult to imagine. It was from the start a work of collaboration, real and not nominal, with each step in the design closely argued. Contention grew hottest (so it is reported) over rainwater disposal: the villagers wanting the usual half-round gutter to be festooned openly round the eaves, the architect demanding—and in the end making it a condition of his continued interest—that it be boxed in. It was.

The reason for this show-down may not be instantly apparent in print, but it is apparent in the flesh or, to be exact, in the wood and stone. For by this means it was possible for the architect to carry the taut lines of machine aestheticism into the wide tolerances of the Cotswold tradition. No floppy hat roof here, discharging with curved belcast into visible ironmongery in the manner Mr. Edwin Gunn's "little things that matter," but a sharp statement, one-roofline-one-plane, with little

functional necessities supplied out of sight—at small added cost.

The little monument is still Cotswold, with traditional methods and materials used throughout, but with whimsicality cast to the wind. The Gloucestershire lad has shaken the straw out of his ears. He's in the army now.

### CITY STATUARY

It is natural in times like the present for cities to look to their statues. In fact, Bristol has been solicitous for hers for some years now, moving the shameful dirt from their faces and, on occasion, changing their positions. Rysbrack's William III was the first to be rehabilitated. Then Neptune was moved from the waste of Victoria Street and put in an all-too-civic garden on the waterfront. Nothing has as yet been done to Edward Colston, who thinks deeply at the bus stop on Colston Avenue, or to Edmund Burke, who delivers his Bristol Address to the traffic which swirls at the intersection of the figure-of-eight on the Tramway Centre—at the very point where men should be least encouraged to listen to him.

But it is in the area of the new Council House that most of the strictly public innovation has taken place. For there are here no less than five newcomers: John Cabot, two unicorns and two groups representing the Spirit of the Severn and of the Avon (or is it the Avon and the Frome?). Of these it is the Unicorns which have had most public success. They are golden—always helpful to a unicorn—and teed up as they are on their pyramidal lead roofs they evidently meet the case. They "punctuate," as the books say. True they are not "what we would do now": but since one of the chief services of public statues is to assert the time dimension, it does not in the long run greatly matter which moment in time they mark.

But no one has so far been found to say a good word for the River Spirits. These are in stone and have less favoured berths, marking projecting spurs of roofs on the rear, convex elevation. It is, alas, only too easy to be funny about statues, but the only way in which these can be briefly described is as small boys surprising giant slugs from the rear. The fault lies, perhaps, less with the sculptor and more with those who set him such a puzzle. It is often charming to freeze the characteristic gesture of a great man, but it is never charming to conjure forth and freeze a thought which has never meant anything to anyone.

John Cabot, by Charles Wheeler, belongs to a different order of feeling to both unicorns and slugs. He is characterized by his fur-edged coatee, his mariner's cap and instrument and his cocky disposition. Round about Christmas he was hurriedly encased in boarding and the police were busy try-



ing to find out who it was who had painted his coat red to look like Father Christmas. The boarding remained up a very long time and the news spread that the paint had bitten so deep into the soft Bath Stone as to be indelible and that, therefore, he would have to keep this genial attribution. But after some months he emerged with only a suspicion of pink about him, of a hue you associate with lingerie.

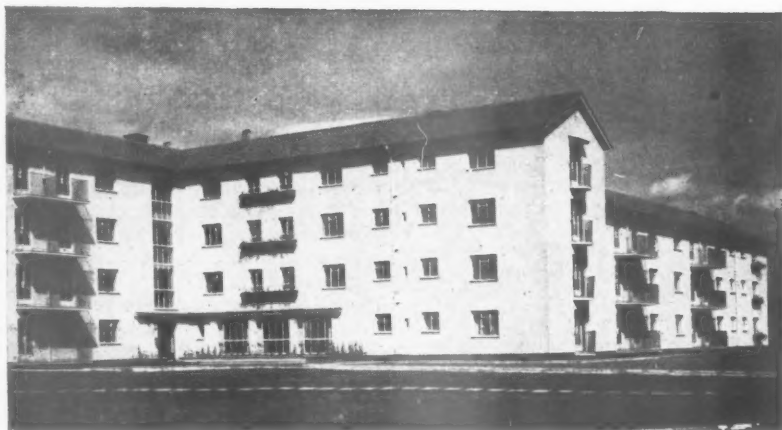
Another statue which has recently made a triumphant come-back on College Green is that of Queen Victoria. This was always thought to be a work of poor quality. But this may have been because she was too closely muffled by trees which, in turn, by discharging depressing green stains over the white marble, gave the impression of a forgotten corner in a cemetery. She disappeared when the trees were grubbed up. But she is back now, cleaned, re-set and majestically alone.

### SCULPTURE IN STORE

All these are in the tradition of civic sculpture. But the chief sculptural event of this Bristol season was perhaps the decision of the Arts Council to loan McWilliam's "Parents and Child" to the new Dolcis shop in Broadmead.

This decision marked, surely, a new departure in sculptural patronage, a setting up of a sort of "salon des refuses." The Fuss—and still more the respectful silences—which have greeted the Unknown Political Prisoner, make it clear that the Public is not prepared to see the recent Art of Analogy taking the place of the customary Art of Representation in the sphere of formal sculpture. Nevertheless, this same Public is becoming more and more receptive of Analogy in Art Forms which are not associated with traditional usage—in strip cartoons, for instance, and in the art of Display. It is in this latter syringe that Analogy is being injected into Bristolians. For this, the aura of the shoe shop is vastly more favourable than that of the Art Gallery: viewers do not go in with the dogged step of seekers after Culture and it does not enter their minds to keep making unhelpful comparisons with Michaelangelo. The danger is rather the opposite: that the solemnity of the thing does not strike home: that in passing they carry away a vague impression that it is only a dummy for exhibiting corsets—even though a close look would have told them that the part which needs corseting isn't there.

But this is a risk which can harm no one. Fears had been expressed—particularly by Mr. Searls, the manager—that the work would act as a Spectre at the Feast and would deter sales. But these fears have not been realized. Though the group strikes a different note to the shop, both use the same language; and it seems that both have been cheerfully accepted.



Flats for Single Women at Glasgow receive Saltire Society Award

*Calfhill Court at Pollok, Glasgow, contains 60 flats for single women, a caretaker's flat of 876 sq. ft., and a common room of 876 sq. ft. Each flat of 427 sq. ft. contains a living-bedroom of 243 sq. ft., and bathroom, kitchen and storage space. Wings are 23 ft. 9 in. wide; average frontage, 21 ft. 6 in. Access to the 18 flats per floor is by three staircases and balcony. Site area, 2½ acres. Contract price, £77,886. Designed by Glasgow City Architect, A. G. Jury; principal housing architect, T. Barclay; assistant architect, T. L. Fleming.*

### BUILDINGS IN THE NEWS

#### First Eastern Region Ticket Office in London

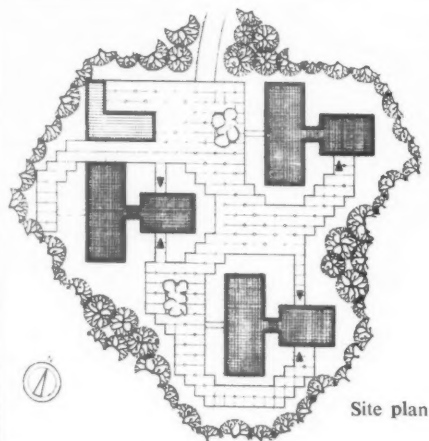
*Below, the ticket and enquiry office in Victoria Street, S.W.1, for the Eastern Region of British Railways. The accommodation is on three floors and is allocated as follows:—Window display, 42 sq. ft.; public area, 352 sq. ft.; office area, 626 sq. ft.; lavatories, mess, stores, air conditioning plant, 927 sq. ft. The maximum number of staff is 10; 4 ft. of counter is allocated per clerk. Assistant in charge, R. G. Robbie.*







Typical floor plan  
[Scale:  $\frac{1}{8}'' = 1' 0''$ ]



Site plan



#### Eleven Storey Flats in Coventry save land

The three eleven-storey point blocks on which work will start in a few weeks at the Tile Hill Neighbourhood, Coventry, contain 33 two-bedroom flats each of 702 sq. ft. Density, approximately 40 units per acre. Construction, no fines concrete. City Architect, Donald Gibson; deputy, F. Pooley; principal, E. C. Tory; chief assistant, G. Grey.



#### Messroom for London Goods Depot

Below, left, a mess for 128 men at the Commercial Road Goods Depot, E.I. The total floor area is 2,316 sq. ft. The messroom is 1,700 sq. ft. in area; the kitchen area of 240 sq. ft. contains a large cooker, high pressure hot water boiler, toaster and gas water heater. The frame spans 28 ft. 9½ in. at 12 ft. centres. Cost, 3s. 2½d. per ft. cube. Assistant in charge, J. Ward.

(The British Railways Eastern Region architect, H. H. Powell, and his principal assistant, R. T. Walters, were responsible for the three railway buildings on these pages.)

#### First Post-War Staff Hostel of British Railways

The staff hostel at Scunthorpe, Lincolnshire, provides living accommodation for 40 men. The total floor area of 13,000 sq. ft. includes 40 cubicles of 60 sq. ft., and games, recreation, hobbies, dining rooms, and kitchen. The cubicle blocks have a span of 22 ft. and bays 14 ft. wide. Single storey construction was adopted for speed of erection. Site area, 58,000 sq. ft. Price, 4s. 5d. per ft. cube. Assistant in charge, A. J. Fagg.





## COMPETITION

### Awards for Hospital at Doha

The first prize of £1,250 in the competition for a state hospital at Doha for the Government of Qatar on the Persian Gulf has been won by John R. Harris. The second prize of £1,000 has been won by Alister MacDonald and Eric Maidment, and the third prize of £750 has been won by Dr. C. L. P. Franck. The competition was assessed by Alexander S. Gray of W. H. Watkins, Gray and Partners. The 74 designs submitted will be on view at the BC, 26, Store Street, W.C.1, until the end of the week.

## CPRE

## Menaces to Beauty of Countryside

The CPRE has asked county branches to report on developments which spoil the countryside. The material to be collected, including photographs, will be presented in five categories: structures, dumps and disorders, excrescences, natural and unnatural objects and miscellaneous. These categories will include information on new buildings, television and radar masts, traffic signs, bus shelters, power stations, grid lines, open-cast workings, lamp posts, bridges, ordnance factories, gasworks, ugly afforestation, paving surfaces, traffic islands, highway dumps, kerbs, and the disfigurement of village greens and ancient monuments.

H. G. Griffin, general secretary of the CPRE, said recently: "We want to collect a dossier to show that the accumulation of these things is really very serious and that we are destroying a valuable national asset."

*Cup for Best Kept Village*

Bourton-on-the-Hill has been awarded the Bledisloe Cup for the best-kept village in Gloucestershire by Lord Bathurst, president of the Gloucestershire branch of the CPRE. The runner-up was Willersley, last year's winning village. Twenty-eight villages competed.

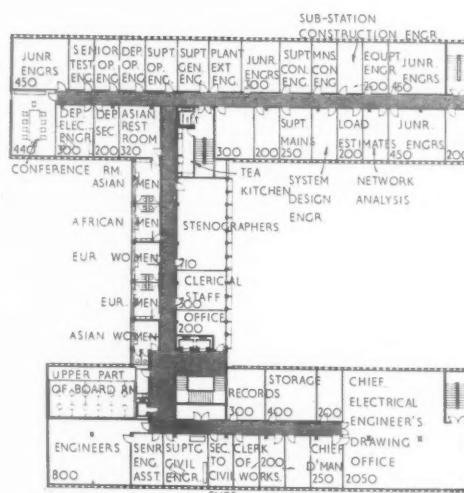
## MOHLG

### *Housing Progress*

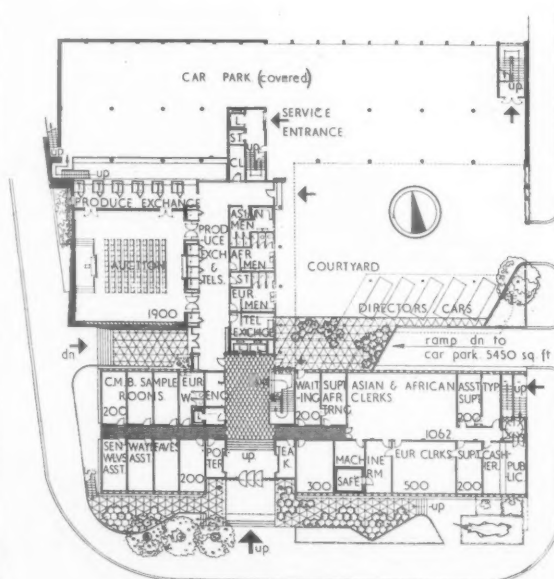
The number of permanent houses completed in Great Britain during July was 27,206, as compared with 26,598 in June and 19,985 in July, 1952. In the first seven

## U K ARCHITECTS WIN SECOND PRIZE IN

The second prize in the competition for the Uganda Electricity Board head office building, at Kampala, Uganda, was won by UK architects Peatfield, Mayo and Bodgener. Their design, illustrated here, consists of two five-storey blocks, facing north and south, with a link containing ancillary accommodation. Car-parking space is provided at basement level of the south block and ground-floor level of the north block. A produce exchange and offices for the Uganda Electricity Board and Coffee Marketing Board occupy the remaining space on the ground floor. Offices for the Limb Marketing Board and Coffee Marketing Board occupy the first floor. The second floor of the south block is allocated to the Uganda Development Corporation. Offices for the Uganda Electricity Board occupy the remainder of the second floor and all of the third and fourth floors. A restaurant is situated on the fifth floor. The floor-to-floor height is 11 ft. The cost of the building would be £350,000 at 5s. 6d. per sq. ft. cube.

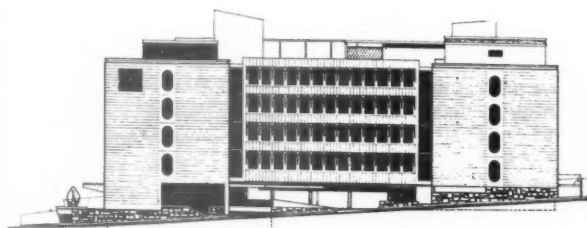


### Third floor plan

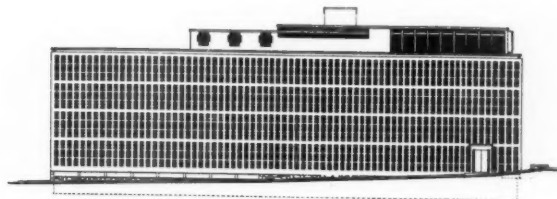


Ground floor plan  
[Scale :  $\frac{1}{8}'' = 1' 0''$ ]

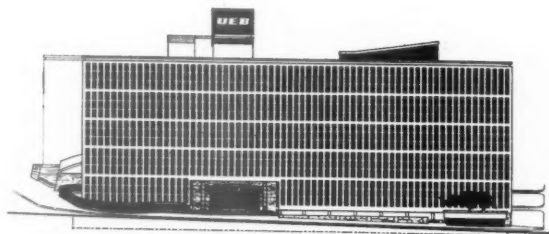
# IN UGANDA ELECTRICITY BOARD HEAD OFFICE COMPETITION



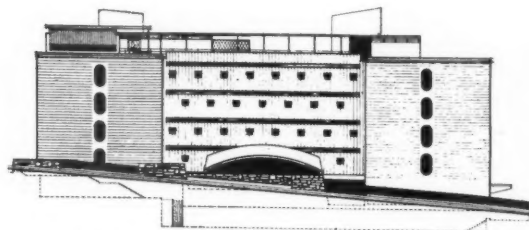
East elevation



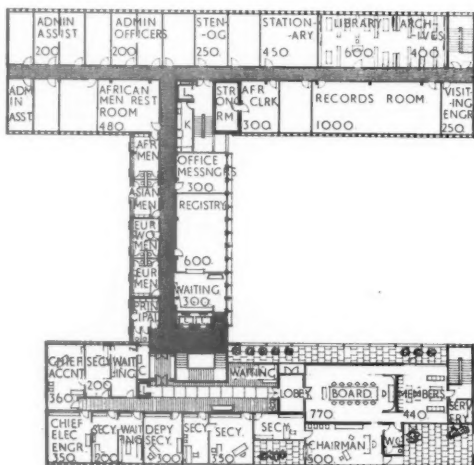
North elevation



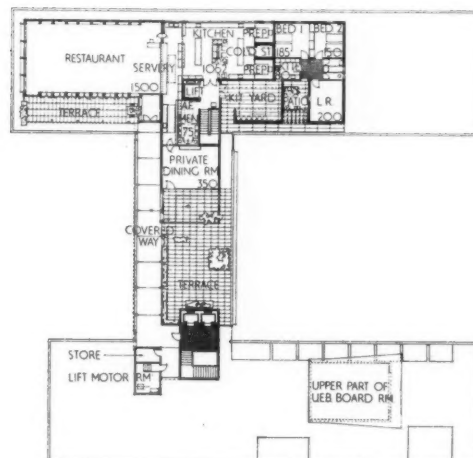
South elevation



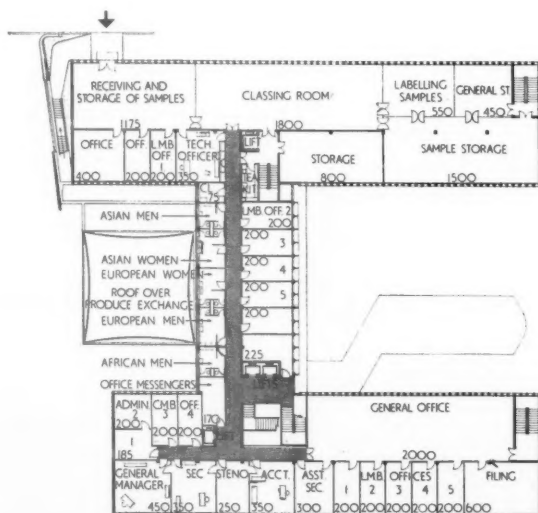
West elevation



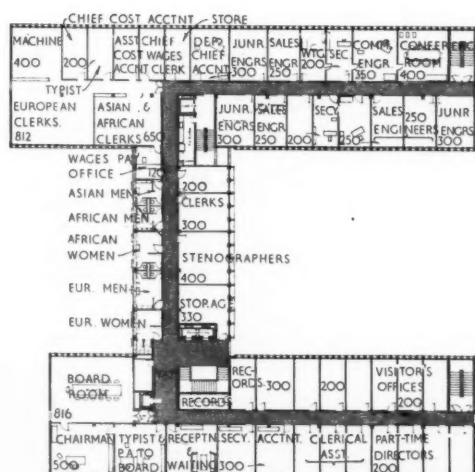
Fourth floor plan



Fifth floor plan



First floor plan



Second floor plan

months of this year, 172,510 permanent houses were completed, as compared with 130,949 in the same period of 1952.

## GLASGOW

### Housing Problem

The Clyde Valley Planning Advisory Committee recommends that the Cumbernauld area in Lanarkshire and Dumbartonshire should be used for building 20,000 houses to accommodate about 80,000 of Glasgow's "overspill" population. This is one of the areas recommended for the development of a new town by Sir Patrick Abercrombie in Clyde Valley development plan.

Of the 100,000 houses that Glasgow hopes to build in the next 20 years, at least 60,000 will have to be built outside the city boundary.

## LAW

### Valuation for Rating

The Valuation for Rating Act, 1953, which came into force on July 31, abandons the methods for the valuation of dwelling houses laid down by the Local Government Act, 1948. Sections 74 to 82 of this Act are repealed. The new Act provides:—(1) The gross value for rating purposes of a dwelling house, private garage or private storage premises is to be the rent at which the premises might reasonably have been expected to let on June 30, 1939. The conditions at the time are taken to be on a year-to-year basis, the tenant paying the usual rates and taxes, the landlord bearing the cost of repairs, insurance and such other expenses as were necessary to maintain the premises in a state to command that rent. (2) The premises are to be valued on the basis that they existed on June 30, 1939, in the state they were, and with the same amenities available, as at the time of the valuation.

(3) Special provisions are included to deal with particular circumstances, namely:—(a) Where the Rent Acts produced an unduly high, or low, rent, or where there were unusual conditions in the locality producing a marked scarcity or abundance of dwelling houses; (b) where the premises are only partly used as a dwelling house; (c) agricultural dwelling houses; (d) where the landlord provided as part of the rent certain services beyond repairs. The Act does not apply to Scotland or Northern Ireland. It leaves the valuation to be made by the Inland Revenue Valuation Dept.

## DIARY

**Charles Rennie Mackintosh.** Exhibition at the Saltire Society, Gladstone's Land, 483, Lawnmarket, Edinburgh. Weekdays, 10.30 a.m. to 12.30 p.m., 2 p.m. to 5 p.m.; Sundays, 2 p.m. to 5 p.m.

UNTIL SEPTEMBER 12

**Home and Surroundings.** RIBA travelling exhibition at the Art Gallery, Public Library, Dudley Road, Tunbridge Wells, Kent.

UNTIL SEPTEMBER 16

**Are Small Market Towns Dying?** A discussion by Thomas Sharp, Geoffrey Clark and John Castle on the BBC Home Service at 9.15 p.m.

SEPTEMBER 17

**Drawings by Robert and James Adam.** At the Iveagh Bequest, Kenwood. Weekdays, 10 a.m. to 6 p.m.; Sundays, 2.30 p.m. to 6 p.m.

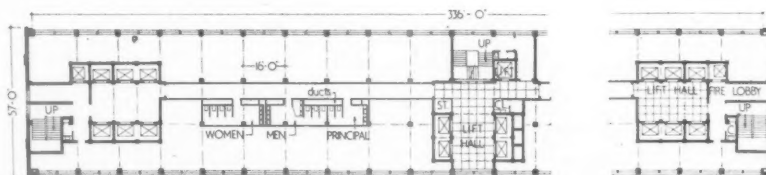
UNTIL SEPTEMBER 27

**Lighting to Measure.** Exhibition illustrating correct use of lighting. At Heal's, 195-99, Tottenham Court Road, W.1. Weekdays, 9 a.m. to 5.30 p.m.; Saturdays, until 1 p.m.

UNTIL OCTOBER 10

## PROJECT FOR BUCKLERSBURY HOUSE

The 132,000 sq. ft. site of the project for Bucklersbury House in the City is bounded by Bucklersbury on the north-east, Walbrook on the south-east, Cannon Street on the south-west, and Budge Row on the west. Section one of the project, illustrated here, has a plot ratio of 5.5 and a gross floor area of 450,000 sq. ft. Section two, which will consist of a wing added to the north-west of the present project, will bring the total plot ratio to 5.75. The central wing, which faces south-east, is 14 storeys high. Caretaker's and engineer's flats are situated at roof level. The sub-basement contains an oil-fired boiler which will supply hot water to ceiling panels. Garage space for 100 cars is provided in the basement. The ground floor will be let to insurance offices, banks, restaurants and two public houses. The building will be steel-framed and a cladding of Portland stone with panels in a darker stone is proposed. There are 20 public lifts. Internal lavatories will have duplicate mechanical ventilation systems. The building has been designed by Owen Campbell-Jones. Bottom, the model from the south; right, from the west. The elevations are still subject to revision, see page 307



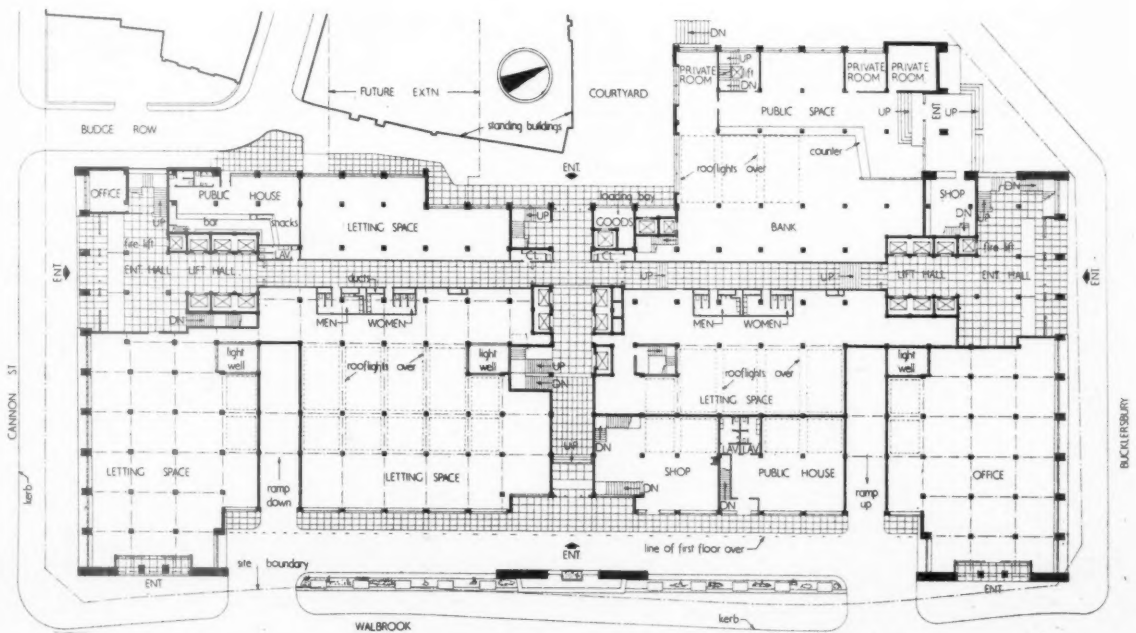
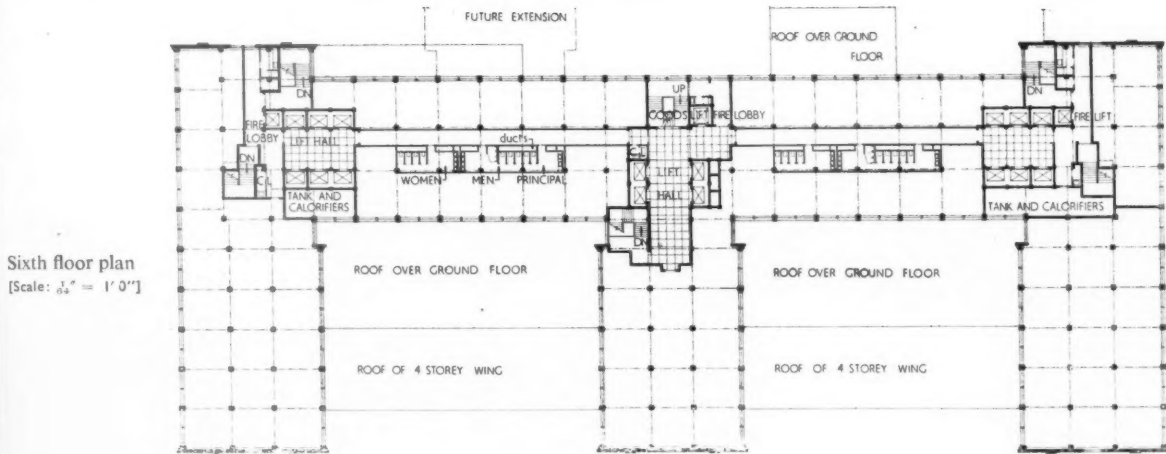
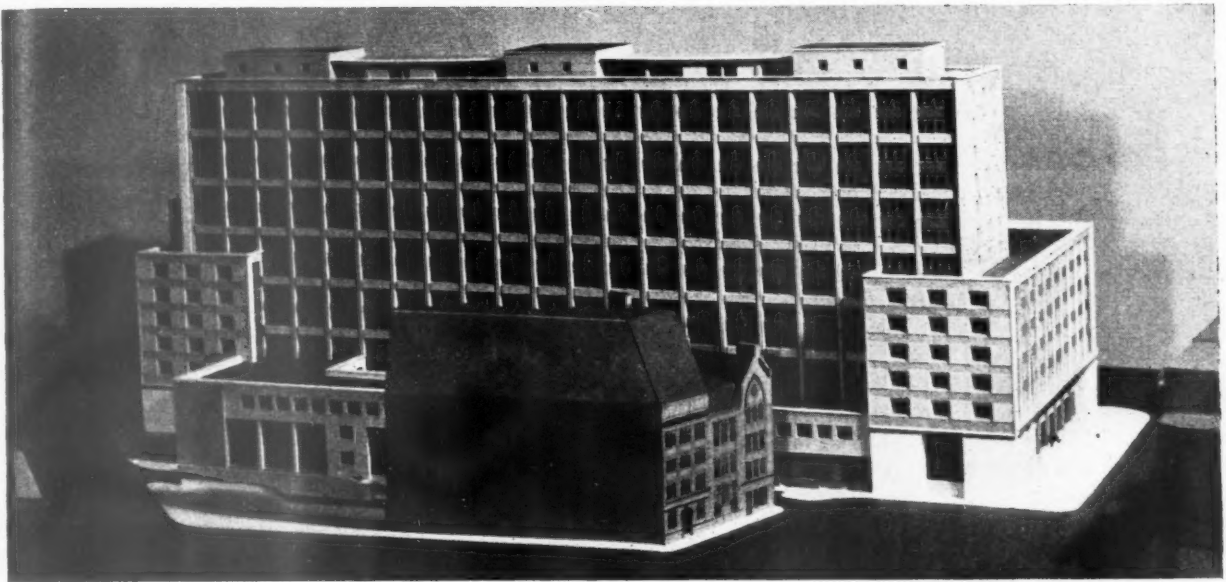
Part eleventh floor plan



Sixth  
[Scale:

Ground  
floor  
plan





## HOUSING

at WESTON GREEN, ESHER, SURREY

architect, ERIC LYONS,

assistant-in-charge, GEOFFREY SCOBLE, consulting engineer, Z. PICK

quantity surveyors, THOMAS BARRETT, SONS and PARTNERS

This housing scheme in Longmead Road, Weston Green, for the Esher Urban District Council, consists of forty 3-bedroom houses, four 4-bedroom houses, eighteen 2-bedroom flats and four old people's bungalows. Provision has been made for 28 garages to be built at a later date. The site has a total area of 10.08 acres, of which 2.8 acres is taken up by a pit on the east side, between the houses and the railway embankment, thus reducing the area available for building purposes.

*Flats and terrace houses from the south-east.*

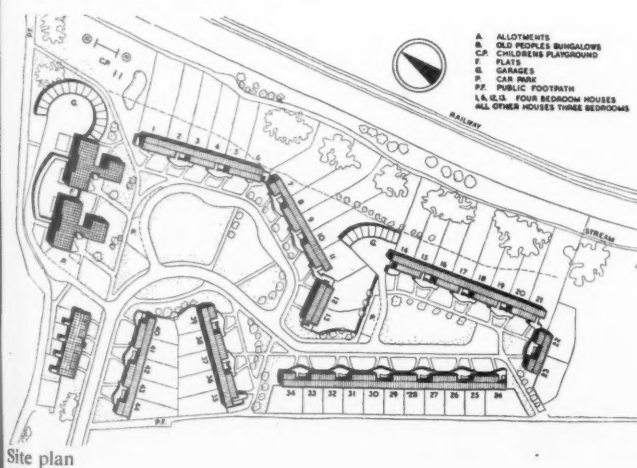


Above:  
terraced  
3-bed  
This  
in the

Site p



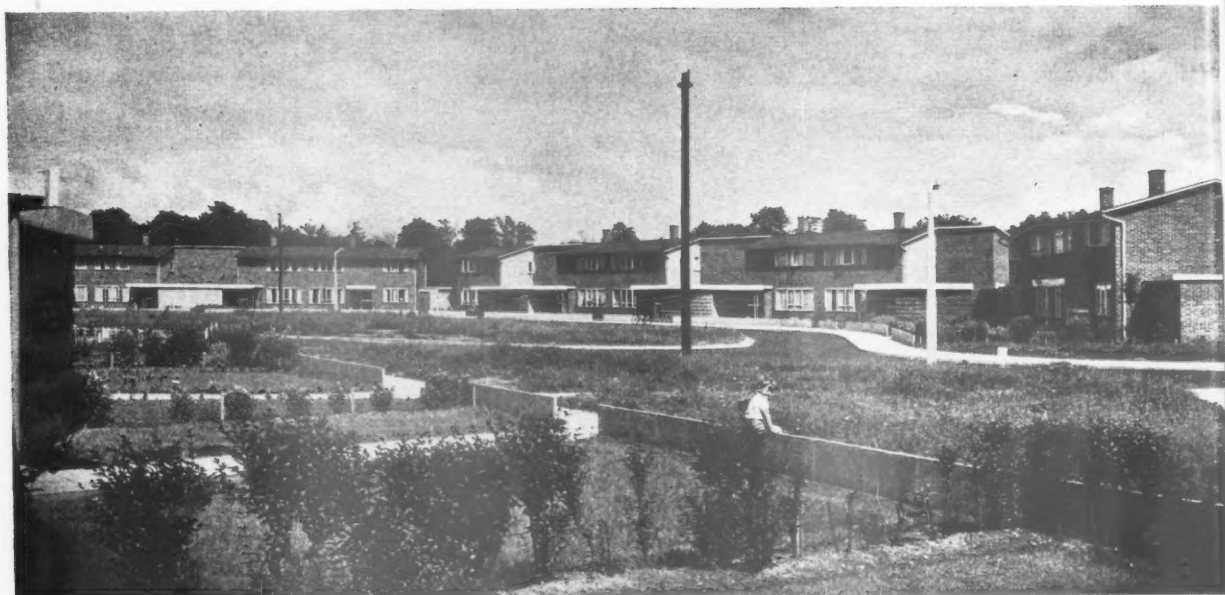
Above, right, a group of 3-bedroom terrace houses. Above, a group of eight 3-bedroom terrace houses from the west. This estate was illustrated as a project in the JOURNAL for November 11, 1948.



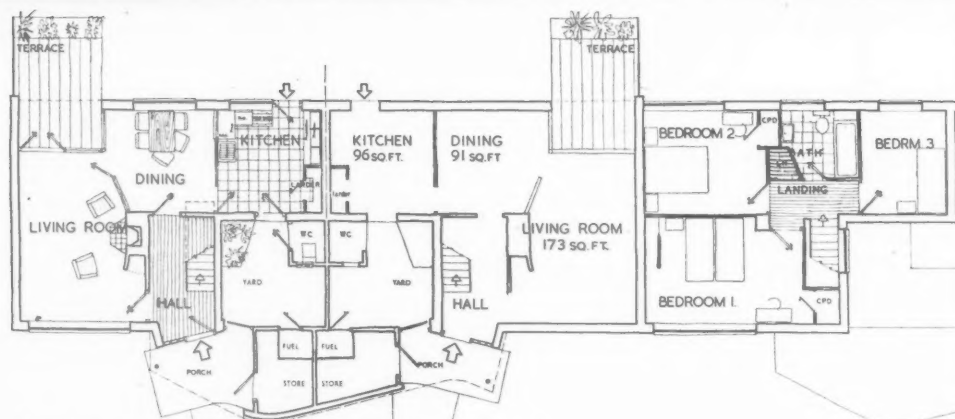
Site plan

**SITE.**—To prevent the railway embankment dominating the estate, the buildings are grouped round two greens. The larger of these forms the focal centre of the scheme, with the 3-storey flats and the bungalows forming a contrast to the blocks of houses. It was originally proposed that no separate front gardens should be provided and that all space in front of the buildings could be developed as common gardens and maintained by the Council. This proposal was finally abandoned by the Council on the grounds of excessive maintenance costs and conventional front gardens were provided. The greens were not intended to be mere grassed areas, as they are now, but were designed to contain fairly dense planting, including forest trees, and it is hoped that planting will be carried out shortly. Rear gardens to homes vary considerably in size, the smallest, those on the west side, are only 40 ft. by 30 ft. Allotments are provided for tenants requiring them.

**PLAN.**—The houses have been planned to suit almost any orientation by the use of a through living room leading to a terrace at the rear. The living room is separated from the dining room by a glazed screen and door. The partition and fitment between the dining room and kitchen is also partly glazed, so that all ground-floor rooms are visually connected. The service yard gives access to an outside w.c., stores and fuel, and kitchen. 4-bedroom houses have another bedroom on the first floor, a study on the ground floor and a different arrangement of outbuildings. The area of 3-bedroom houses is 966 sq. ft. and 4-bedroom, 1,247 sq. ft. The flats were planned in T-shaped blocks for economy,



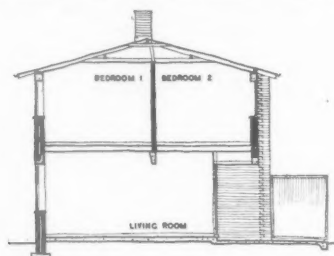
Above, 2-storey terrace housing from the south-west. Centre left and extreme right can be seen examples of the 4-bedroom houses.



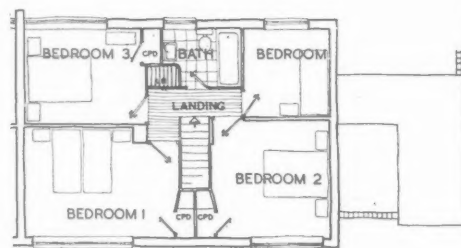
Ground and first floor plans, 3-bedroom house [Scale:  $\frac{1}{16}'' = 1' 0''$ ]

## HOUSING

at WESTON GREEN, ESHER,  
SURREY  
architect, ERIC LYONS

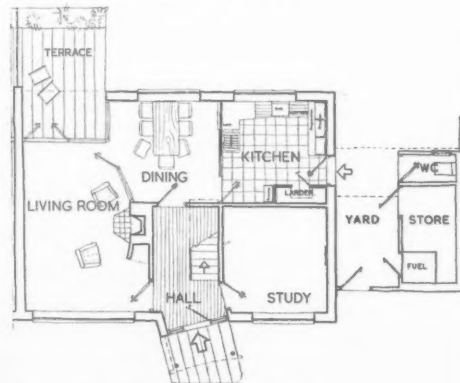


Typical cross-section



First floor plan [Scale:  $\frac{1}{16}'' = 1' 0''$ ]

to create enclosures giving some privacy to ground floor flats and to make possible the retention of a number of orchard trees. The large living room divides easily into sitting and dining areas, with access to the kitchen. One bedroom is entered directly from the living room and is also linked to the second bedroom. The provision of double doors makes it possible to use the first bedroom "en suite" with the living room. The old people's bungalows are planned with a minimum of circulation space and with a partly-covered porch and a bay window, so that the occupants can have the

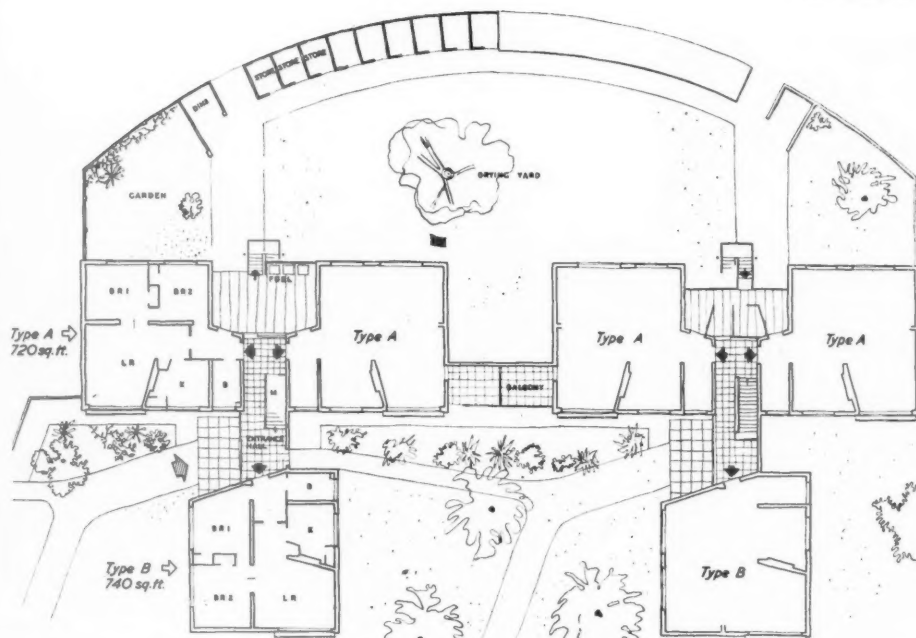


Ground floor plan, 4-bedroom house



## HOUSING

at WESTON GREEN,  
ESHER, SURREY  
architect, ERIC LYONS



Ground and typical upper floor plan of flats [Scale:  $\frac{1}{32}'' = 1' 0''$ ]

Right, main entrance to the easterly block of flats. Below, the two linked blocks of flats from the west.



opportunity to observe some outside activity. Area of each bungalow, excluding porch, 410 sq. ft.

**CONSTRUCTION.**—Houses have external walls of 11-in. cavity load-bearing brickwork and 4½-in. load-bearing brick partitions. Walls to stores are of 4-in. precast concrete blocks. Ground floors are of 5-in. concrete and first floors of 6-in. by 2-in. timber joists. Roofs are of timber trusses at 2 ft. centres. External wall construction of the flats is 11-in. cavity brickwork and 9-in. rendered or tiled brickwork. Old people's bungalows have similar construction to the houses, except store walls of 4½-in. rendered brickwork and roofs of hollow block r.c. slabs.

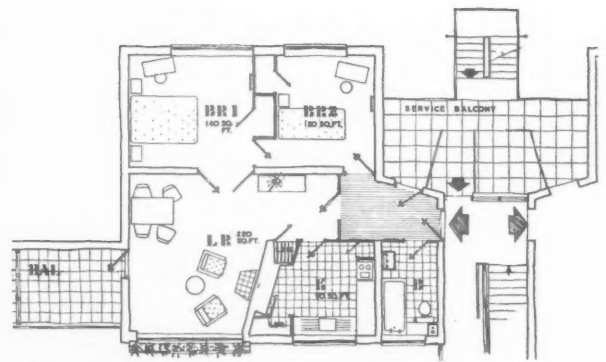
**FINISHES.**—House roofs are of 26-gauge copper on ½-in. insulation board and ceilings are of plaster-board. There is 1-in. of wood wool under bathroom floors as sound insulation. Floors are finished with ½-in. coloured mastic asphalt to ground floor rooms, quarry tiles in kitchens and first floors have ¾-in. boarding. Walls are plastered internally. Non load-bearing partitions are of 2½-in. clinker blocks. Finishes are mostly similar in the flat blocks except that floors and roofs are insulated with 3-in. foamed slag screed and the roof has 3-ply built-up felt. Floors in stair halls are covered with coloured granolithic. Windows are EJMA timber sashes throughout.

The contract prices for the 44 houses was £65,750. This is approximately £1,350 for 3-bedroom houses and £1,650 for 4-bedrooms. The price per ft. cube

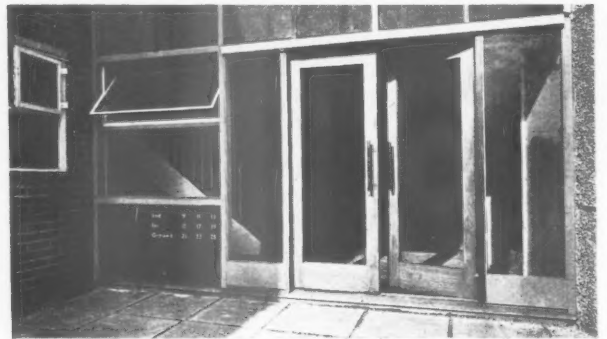




*Above, looking north at the balconies which link the two T-shaped blocks of flats. Above, right, main entrance doors in the westerly flat block.*



Plan of typical type A flat [Scale:  $\frac{1}{16}'' = 1' 0''$ ]



## HOUSING

in WESTON GREEN, ESHER, SURREY  
architect, ERIC LYONS

is 2s. 11d. for 3-bedroom and 2s. 8d. for 4-bedroom, and 30s. 3d. and 26s. 7d. per ft. sq. respectively. For the 18 flats the contract price was £26,575. This is 3s. 10d. per ft. cube and 39s. per ft. sq. The contract price for the 4 old people's bungalows was £4,500. This is 3s. 6d. per ft. cube and 44s. per ft. sq.

The general contractors for each contract were H. B. Kingston Ltd. For sub-contractors see page 334.

*Right, four old people's bungalows to the north of the approach road to the site. In the background is one of the blocks of flats. These bungalows will be illustrated in more detail in a later issue of the JOURNAL.*



The Hunstanton school, now nearing completion, will provide secondary school facilities in a large rural area. In addition, it will serve as an evening institute (it has the only decent hall in the district) and a gardening centre for the neighbouring primary schools. Designed by Alison and Peter Smithson, for the Norfolk Education Committee, it stands on a 22-acre site on the top "edge" of Hunstanton—an early Victorian seaside resort. It is planned so that formal teaching areas have south aspect and Sandringham prospect; practical rooms have north aspect and little prospect; "contemplative" rooms face east and west on to the enclosed courts. The plan is unusual and the structure—welded steel H-frames—is one of the first in this country to be designed according to the "plastic theory." Both plan and structure were dealt with at the discussion between the architects (left and centre in photo) and the engineer and Editors of the JOURNAL, recorded below. The design of the school was the subject of a competition won by the architects in 1950. It will be described and illustrated in detail when completed.



## SECONDARY SCHOOL AT HUNSTANTON

A discussion between the architects (Alison and Peter Smithson), the engineer (R. S. Jenkins of Ove Arup and Partners) and Editors of the JOURNAL, referred to below by their respective numbers (see page 305).

**THIRTEEN :** Could you give a brief outline of your approach, to this problem in terms both of plan and structure?

**THE ARCHITECTS :** The problem was to provide a three-stream mixed county modern secondary school for 450 children, with additional accommodation for primary and adult education.

Our solution was to concentrate the normal teaching into one compact shape, a rectangle with three internal courts, the centre one being roofed to form the assembly hall. The impact-noisy elements are separated out and the whole arranged on a raised rectangular podium which dominates the site.

The teaching rooms are on the upper floor, which is eleven feet high, and all the circulation, administration and services are on the lower floor, seven feet eight inches high. Access is by stairs between pairs of (or groups of three) classrooms, with formal teaching and experimental and practical rooms grouped together.

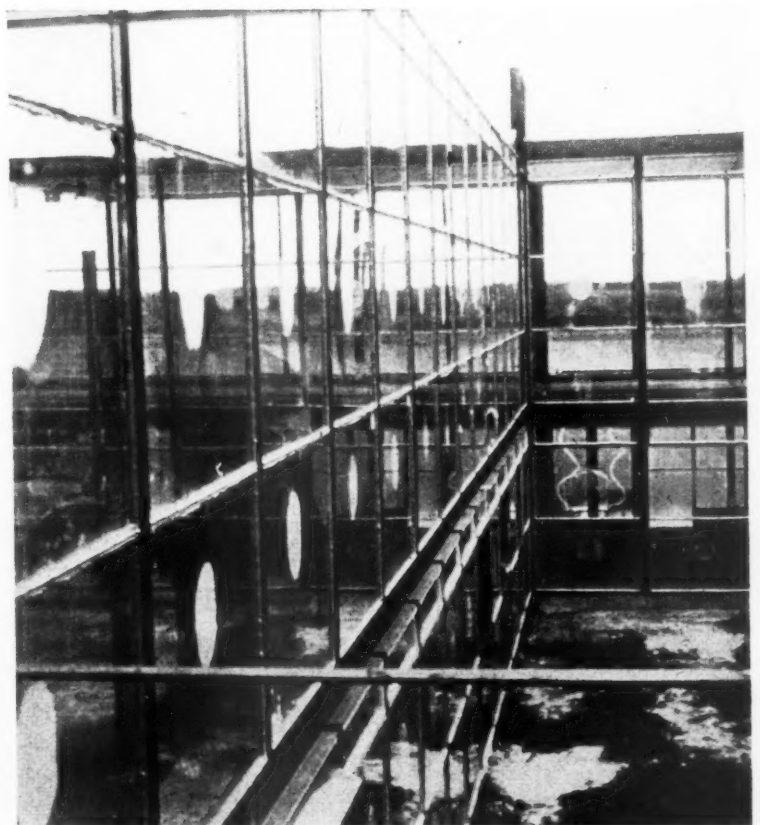
The core of the school is the assembly hall, into which flow the dining and entrance areas, carrying into the school the planes of the forecourt, green-courts and playing fields. This allows the circulation of the hall, the dining areas and the school generally to be superimposed, resulting in a compact and economical plan.

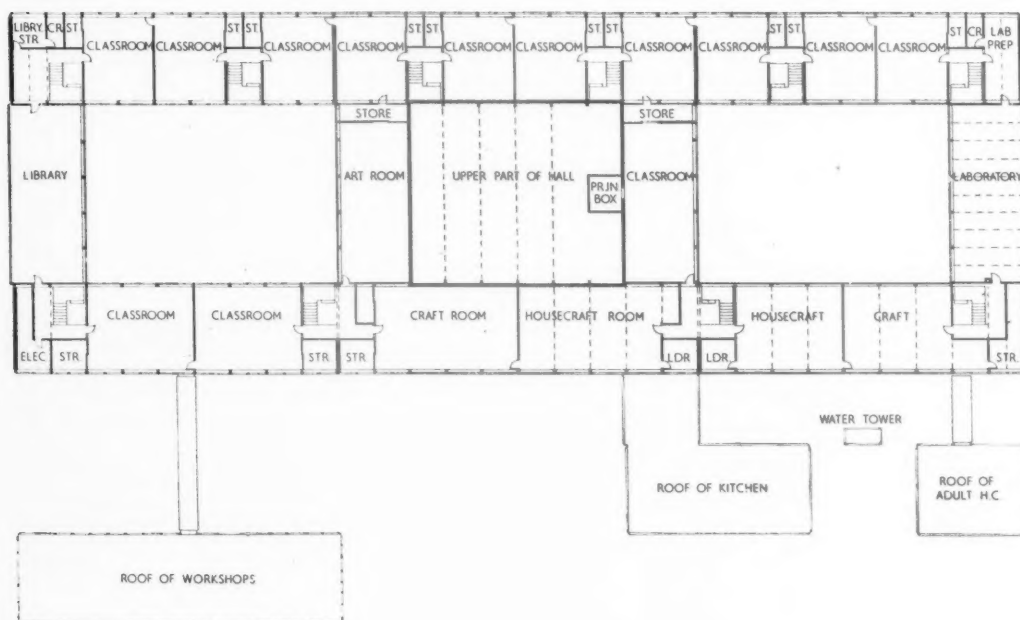
Structurally, our aim was to use steel as an architectural material, but to avoid the formalism of Mies van der Rohe. Each piece of steel supports, defines and decorates the space it creates. For example, facing frames act as jigs and stiffeners to the main frames, as mullions and transoms to the windows, and as fascias and copings to the floors and roofs. The whole being fabricated from standard section rollings and plate pressings.

**THE ENGINEER :** Our "terms of reference"

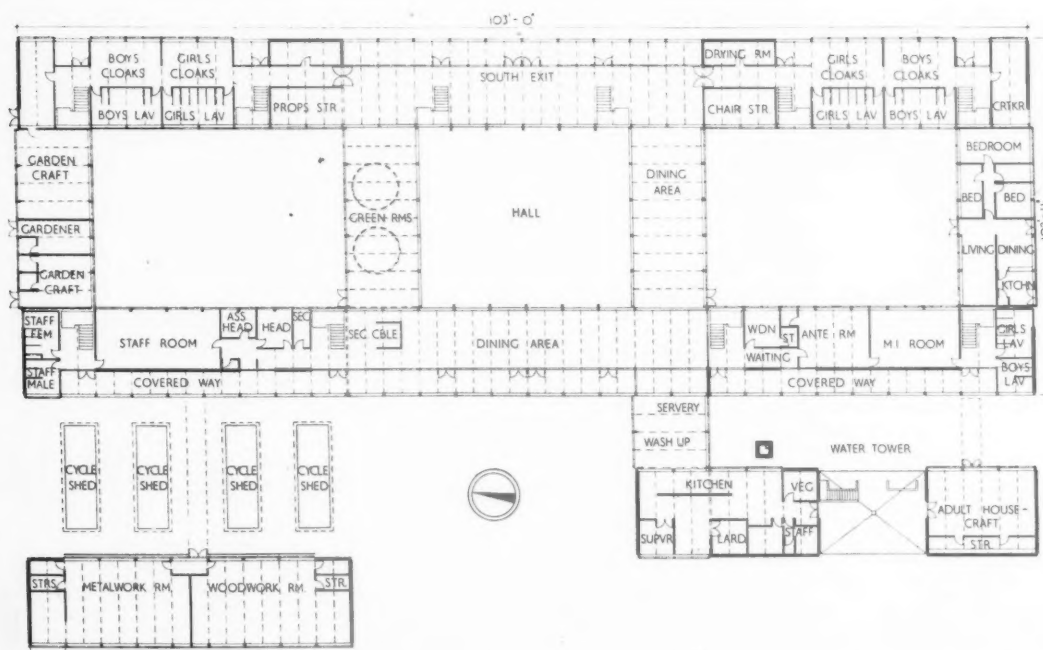
were that the architects wanted to use steelwork "architecturally"—to make the structure contribute architecturally to the whole concept. They wanted

Below, one of the internal, "green," courts. N.B., the floor-to-ceiling glazing.





### First floor plan



Ground floor plan  
[Scale :  $\frac{1}{4}'' = 1' 0''$ ]

exposed steelwork, not cased in, and a structure that would be neat and tidy, that is without brackets bolted on, etc. This pointed towards welded steelwork and this in turn led to our decision to use the plastic theory, which has the advantage over classical design in that it saves steel.

**TWENTY-FOUR:** Does this apply particularly to welded steelwork?

**THE ENGINEER :** Yes, to use the plastic theory to the greatest advantage needs a fully continuous frame.

**TWENTY-FOUR :** Why was so much welding done on the site?

**THE ENGINEER :** Because the frames would have been too large to transport, and because the frame consists mainly of *elements*, i.e., rolled steel sections, not light lattice steelwork. The rolled sections are slightly heavier, but they are cheaper.

**THIRTEEN:** Was it your intention to produce a system of construction applicable to school building generally or did you set out to solve only the problems inherent in this plan?



**THE ARCHITECTS:** The system of construction is a result of our conception of the requirements of this particular school. It is, therefore, unique to this school.

**TWENTY-FOUR:** Would, say, twelve-off not reduce the price per school?

**THE ARCHITECTS:** I doubt it, because each school is built at a different place and at a different time. The whole world "climate" is constantly changing, and one's own development is constantly changing too. At one time you feel you have to do something; a year later you feel the need to do something different. It is true the problem doesn't change but your knowledge of the problem changes.

**THE ENGINEER:** While there is a lot in what the architects say, speaking as an engineer, I think this principle would have a wide application in schools with a regular type of plan, and it could well be applied to other types of building too.

**TWENTY-FOUR:** You would like to see more buildings like this?

**THE ENGINEER:** Yes. If ten or twenty schools like this were built, we would then have a basis for comparison with other systems of school construction.

**THIRTEEN:** Having used a welded-steel frame, designed in accordance with the "plastic theory," do you claim that this has resulted in appreciable economy in either cost or materials?

**THE ARCHITECTS:** The cost of fabrication was high, over £100 per ton, but we were not just making a frame, we were making the whole building—the fabricators were making architecture. You might compare our system of building with mediæval building; we are using steel in the same way as mediæval builders used wood. They chamfered their wood and determined the positions of their pegs in order that their framing members should look well and contribute to the design. Our steel frame has also been designed to contribute to the architecture of the building. But the engineer can tell you more about the cost of the structure than we can.

**THE ENGINEER:** It was our job to make the architects' conception come within the price limit, not between us to design the cheapest possible school. Comparisons of this nature mean very little, but the cost of a fully continuous welded-steel frame worked out on the plastic theory may well be less than a concrete frame or a bolted-steel frame; in the first case because of the saving in weight, in the second because of the saving in steel. The figure of £100 per ton covers all the steelwork, not just the main framing of rolled-steel sections, which was very cheap. The rate per ton was pushed up by the light steelwork of the facing frames, but we must take into account the fact that they also act as the window frames, since the glazing is fixed directly to them.

**NINE:** Though the building is still unfinished, the precision of its structure and detail is already very noticeable. But, in order to have the face of the stanchions and the face of the brick panels in the same plane, you had to have rebates cut in the bricks with a carborundum wheel. How do you justify this expense?

**THE ARCHITECTS:** Nine inches was the optimum depth of the stanchions; we wanted our brickwork

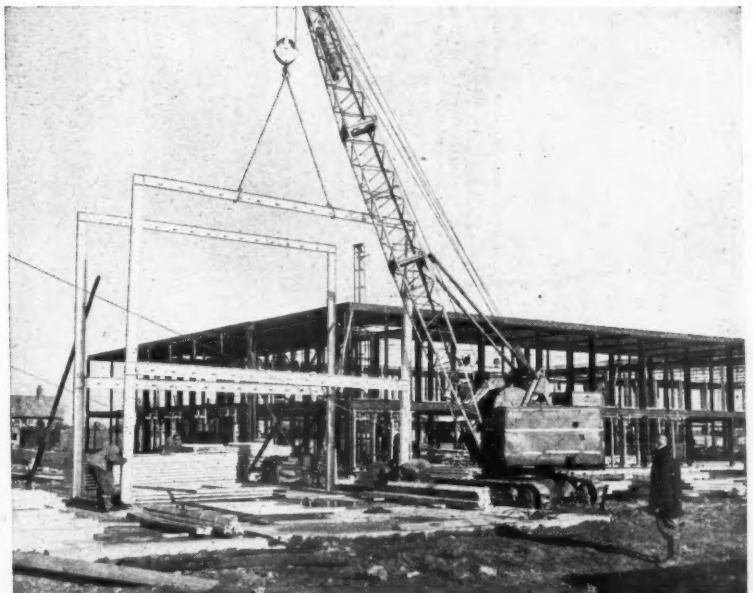
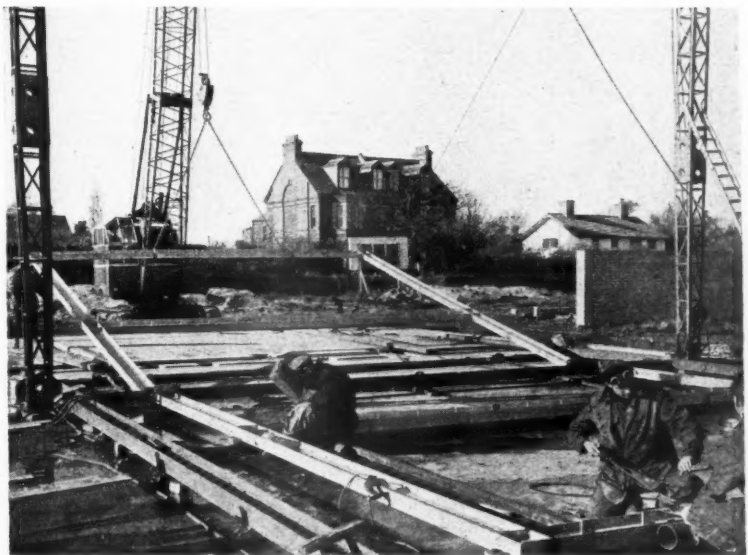
to fit within the flanges of the stanchions and this was the only solution.

**TWENTY-FOUR:** Could you not have used thinner, lightweight blocks for the inner skin; or brick on edge? Or you could have had cavity brickwork, with a wide cavity and the inner skin placed on the inside face of the stanchions.

**THE ARCHITECTS:** Believe us, we considered every possible alternative; cutting these rebates in

#### THE MAIN FRAMES

From top to bottom—(1) a group of rolled steel sections; (2) these sections being welded together on the site jig (in the background a frame is being turned over for downhand welding); (3) a main frame for the gymnasium being hoisted into position (in the background, the main school).





*Above, nearing completion; the south-west corner of the school, with the adult housecraft room on the left. In the foreground are some of the components of the facing frames. Left, a view, looking north, showing (right) the main school building, (left) the adult housecraft room and (centre) the steel framework for the water tower.*



the bricks was the only one that gave us the effect we wanted—both internally and externally.

**TWENTY-FOUR :** This raises the old question—why use brickwork in a steel-framed building, where its load-bearing properties are not needed?

**THE ENGINEER :** The stiffening effect of the brick panels *was* taken into account in designing the steelwork.

**THE ARCHITECTS :** The brickwork helps to give the building that appearance of permanence that so many prefabricated schools lack. A feeling of permanence should be a very important factor in the environment of a child. The “lightness” which so many people admire in “contemporary” architecture may not necessarily be what we need. Permanence is epitomized for us by the “Vanbrugh entrance hall”—the quality of *real materials*. The old board school buildings which are generally condemned today, and allowed to fall into decay, *did* provide this feeling of permanence. We stress this because we want to reverse the present trend.

**SIXTEEN :** Your ingenious nine-inch brick panel (it consists of two 4½-in. skins, separated only by a vertical waterproof membrane) allows you a fair face both outside and inside the building, but it seems to overlook the requirements of thermal insulation. An external panel of this type, combined with large

areas of glass from floor to ceiling, must surely result in considerable heat loss, an expensive heating installation and heavy fuel consumption.

**THE ARCHITECTS :** The brick cladding is mainly to "uninhabited" rooms—stores and so on. Heat losses will be low in any case, because of the compact planning, and we have used double glazing on all north and east exposures. In the courtyards this cuts down "cross-courtyard" sound.

**FIFTEEN :** In forming both floors and roofs of trough-shaped prestressed floor units, with exposed soffits and no suspended ceilings to the rooms below, how have you dealt with sound transmission through the floors?

**SIXTEEN :** And heat transmission through the roof?

**THE ARCHITECTS :** There are no teaching rooms on the ground floor, so the question of sound insulation in the first floor slab does not arise. The roof is insulated with three inches of cellular concrete. **SIXTEEN :** Why did you find it necessary to use both floor panels and forced convection for the heating installation?

**THE ARCHITECTS :** We used floor panel heating for a number of reasons: Firstly, radiators look ugly when placed against glass walls. Secondly, the one heating membrane heats both storeys, as not much heating is required on the ground floor. The ratio is 25% down and 75% upwards. Thirdly, the piping is neat—all you see are two  $\frac{3}{4}$ -in. risers. The forced convection heating we have used only in the very large areas; that is, the hall and the gymnasium.

**THIRTEEN :** With solid walls and partitions and no cavity between floor and ceiling, it is remarkable how little is seen of the services. How have you achieved this?

**THE ARCHITECTS :** By very careful planning; the drawings of all specialists went through our hands and as a double safeguard our clerk of works supervised the installation of pipe runs, etc., particularly carefully.

**NINE :** Have you, in fact, saved space by planning most of the teaching accommodation on the first floor? Though circulation on the first floor seems to have been reduced by the grouping of classrooms in pairs, it has resulted in the provision of ten staircases. Also, on the ground floor, a greater area of external covered space than is strictly necessary seems to have resulted.

**THE ARCHITECTS :** It is true we have ten staircases, but since this is only a two-storey school we have only the number of flights that would be needed by five staircases in a three storey school, and so on. The covered space costs nothing, the frame is there and the cost of the external walling is the same wherever it is placed.

**NINE :** The relationship of dining area and assembly hall is most economical in circulation, but seems to result in loss of definition of the assembly hall as a space. Do you feel this?

**THE ARCHITECTS :** No. The definition of the assembly hall as a space cannot be seen from the plan; there is a considerable change of height from 8 ft. to between 21 and 24 ft. as one enters the hall. When necessary the hall will be cut off from the circulation space by curtains.

**FIFTEEN :** A background of curtains will not be

very good acoustically when the hall is used for the performance of plays and music.

**THE ARCHITECTS :** It is too early as yet to judge the acoustic qualities of the hall, but we believe it will be found excellent.

**NINE :** Architecturally the use of floor to ceiling glazing is most impressive, but in providing it on both sides of most classrooms do you not think there is the possibility of considerable distraction of one class by another across the courtyards, both visually and acoustically?

**THE ARCHITECTS :** It is not a prison; it is a school!

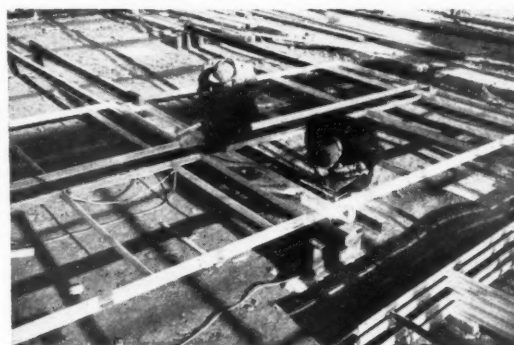
**SEVENTEEN :** As you are using a high proportion of pale yellow gault brickwork and black



Close-up showing precise junction between brickwork and steelwork.

#### THE FACING FRAMES

Right, welding-up factory galvanized components (on the site) to form the facing frames. Below, hoisting them into position. The glazing is fixed directly to these frames.







#### THE FLOOR AND ROOF UNITS

*Above, grouting between the floor units, before the installation of the heating coils. There is no ceiling to the ground floor, the trough-shaped units being left exposed. Left, pre-stressed trough-shaped units being lifted off hoist at roof level.*

*Right, one of the 10 staircases, partially completed. The brick enclosing wall, in the foreground, will not be plastered. Below, close-up, showing contrast between the black-painted steelwork and the floor-to-ceiling glazing.*



painted steel, both inside and outside the building, will you explain something of your proposals for the use of colour on other surfaces?

**THE ARCHITECTS:** We have tried to avoid the somewhat arbitrary use of colour which is so common today. As far as possible, we have used materials (hardboard, plaster, brick, etc.) in their natural colours. We have used painted colour only for specific functions, such as opening lights, doors, services, etc.—to give identity where identity is due.

**THIRTEEN:** When do you expect to finish the building? How long will it have been under construction, after deducting the time lost through non-availability of steel?

**THE ARCHITECTS:** We expect to finish the building by December this year, so that the total construction time will be 20 months, which is the time originally allowed in the contract.

**THIRTEEN:** How long would it take to erect this frame under normal circumstances? Does not the time of erection of the two-skin wall panels compare unfavourably with this and point to the necessity of developing some larger unit with, preferably, dry assembly?

**THE ARCHITECTS:** Normal, traditional techniques, such as the walls, kept up well with the erection of the steelwork which took a total of ten months. With bolted steelwork or prefabrication you need large panels, but ours is a "traditional" method of building.

"Architecture" takes time in any age!

(Main contractor, F. W. Shanks, Ltd.; steelwork, Boulton & Paul, Ltd.)



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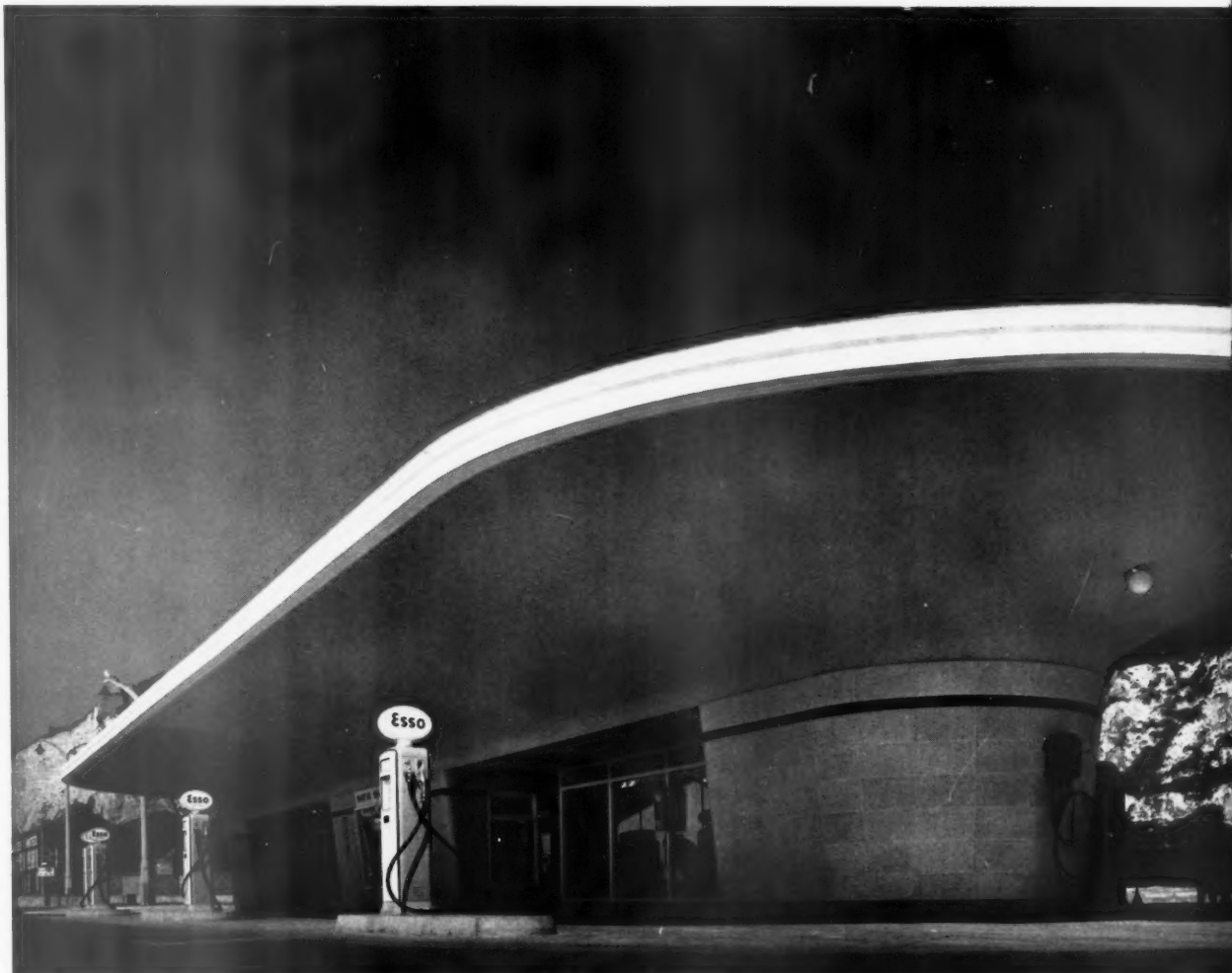




**WORKING DETAIL**

**ROOF: PETROL FILLING STATION AT DOVER**

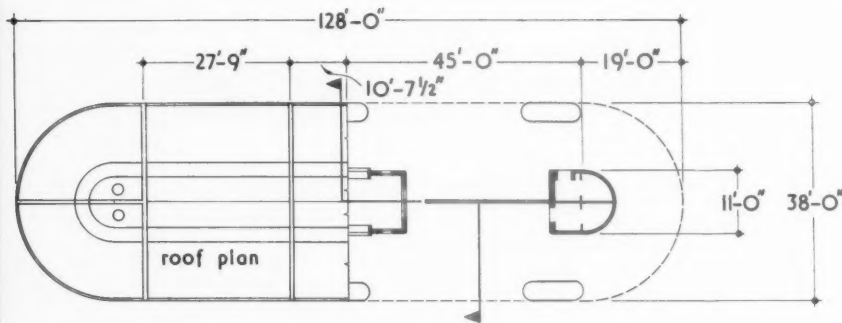
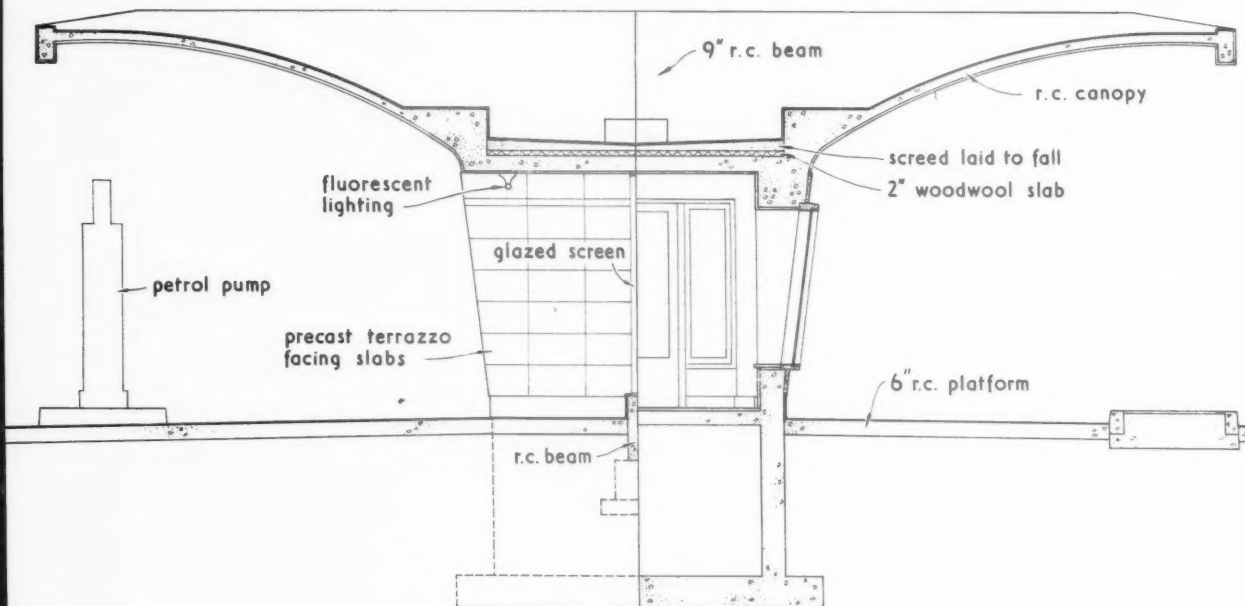
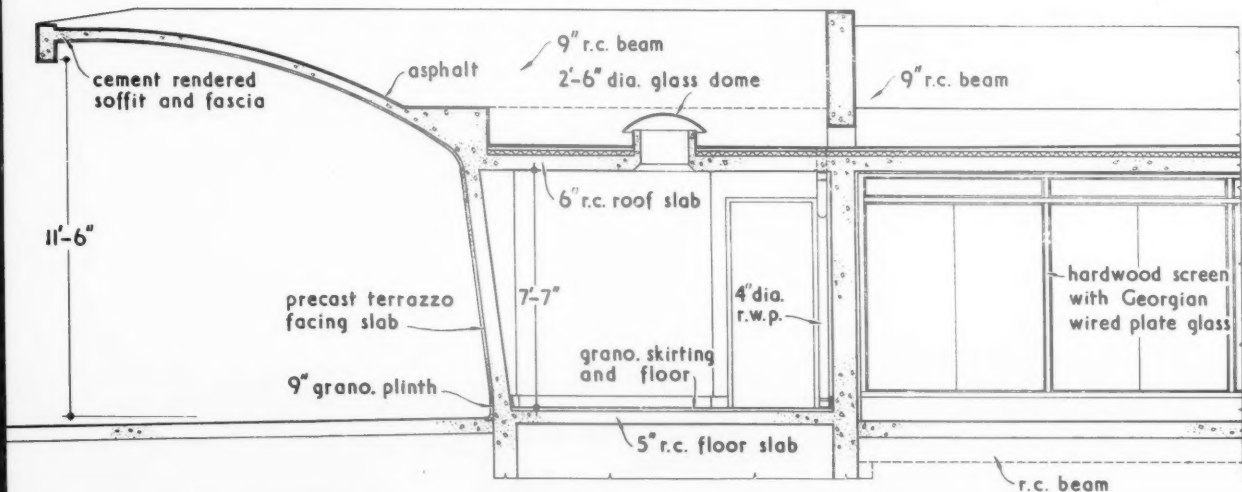
*J. M. Wilson, H. C. Mason and Partners, architects*



*The curved concrete roof overhanging the petrol pumps is supported by cantilevered beams above the roof surface.*

**WORKING DETAIL**

ROOF: PETROL FILLING STATION AT DOVER

*J. M. Wilson, H. C. Mason and Partners, architects*KEY PLAN scale  $\frac{1}{32}'' = 1'-0''$ CROSS SECTION. scale  $\frac{3}{16}'' = 1'-0''$ LONGITUDINAL PART SECTION. scale  $\frac{3}{16}'' = 1'-0''$



# WORKING DETAIL

BALCONIES : 7

BALCONIES: FLATS IN LONDON, S.W.10

*Edward Armstrong and Frederick MacManus, architects ; D. J. Tricker, assistant-in-charge*



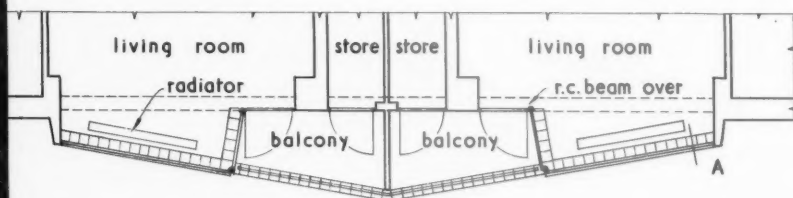
*The facing to the balcony walls is of matt-glazed tiles with a waterproof pointing in the wide joints.*

# WORKING DETAIL

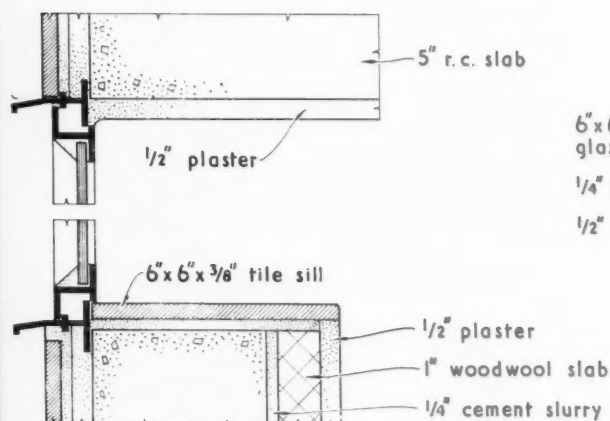
BALCONIES: FLATS IN LONDON, S.W.10

Edward Armstrong and Frederick MacManus, architects; D. J. Tricker, assistant-in-charge

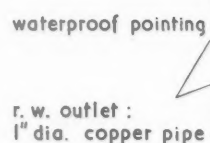
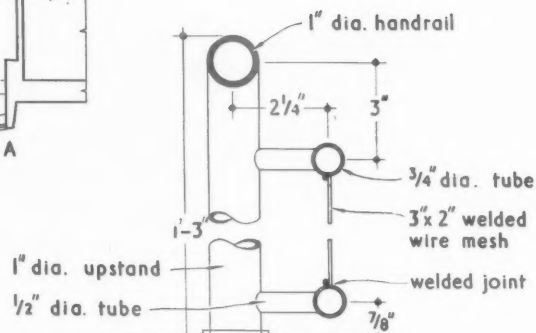
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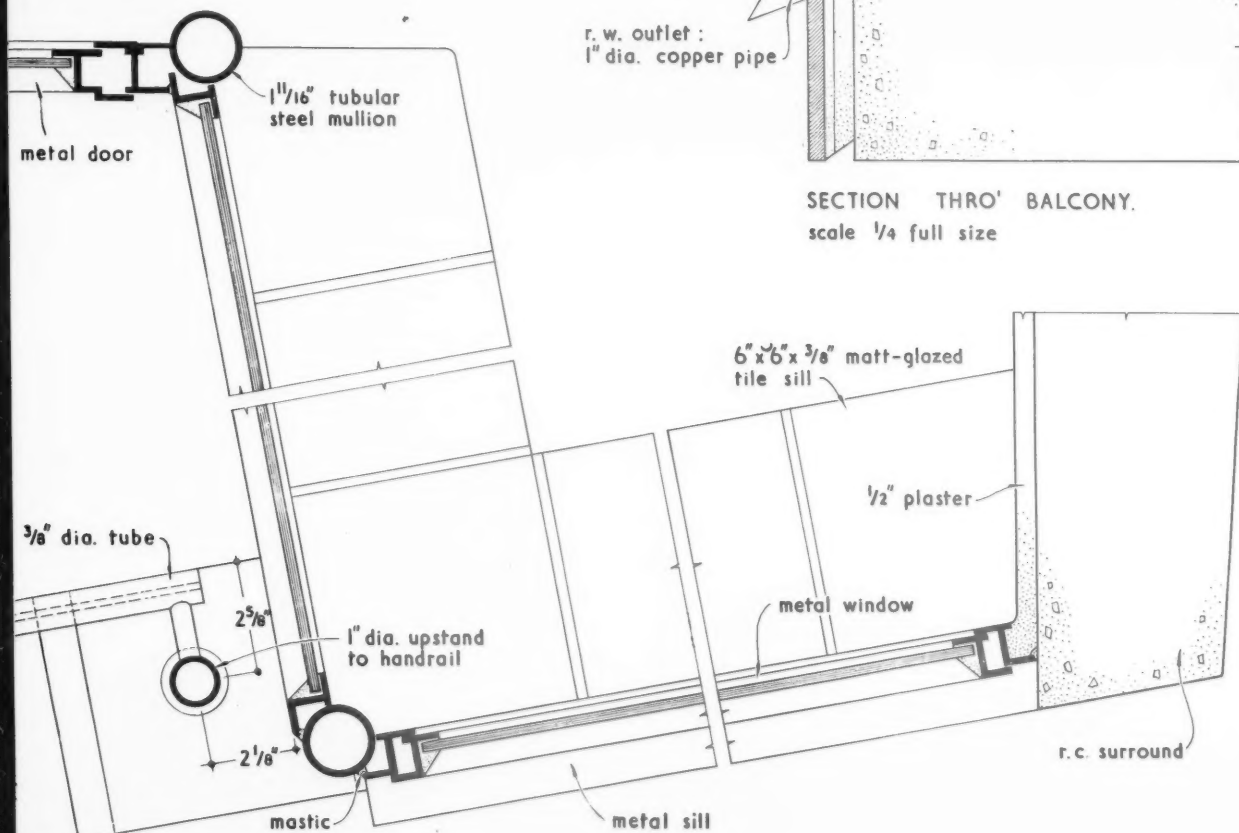
KEY PLAN. scale  $\frac{1}{8}'' = 1'-0''$



SECTION THRO' WINDOW AT A.  
scale  $\frac{1}{4}$  full size



SECTION THRO' BALCONY.  
scale  $\frac{1}{4}$  full size



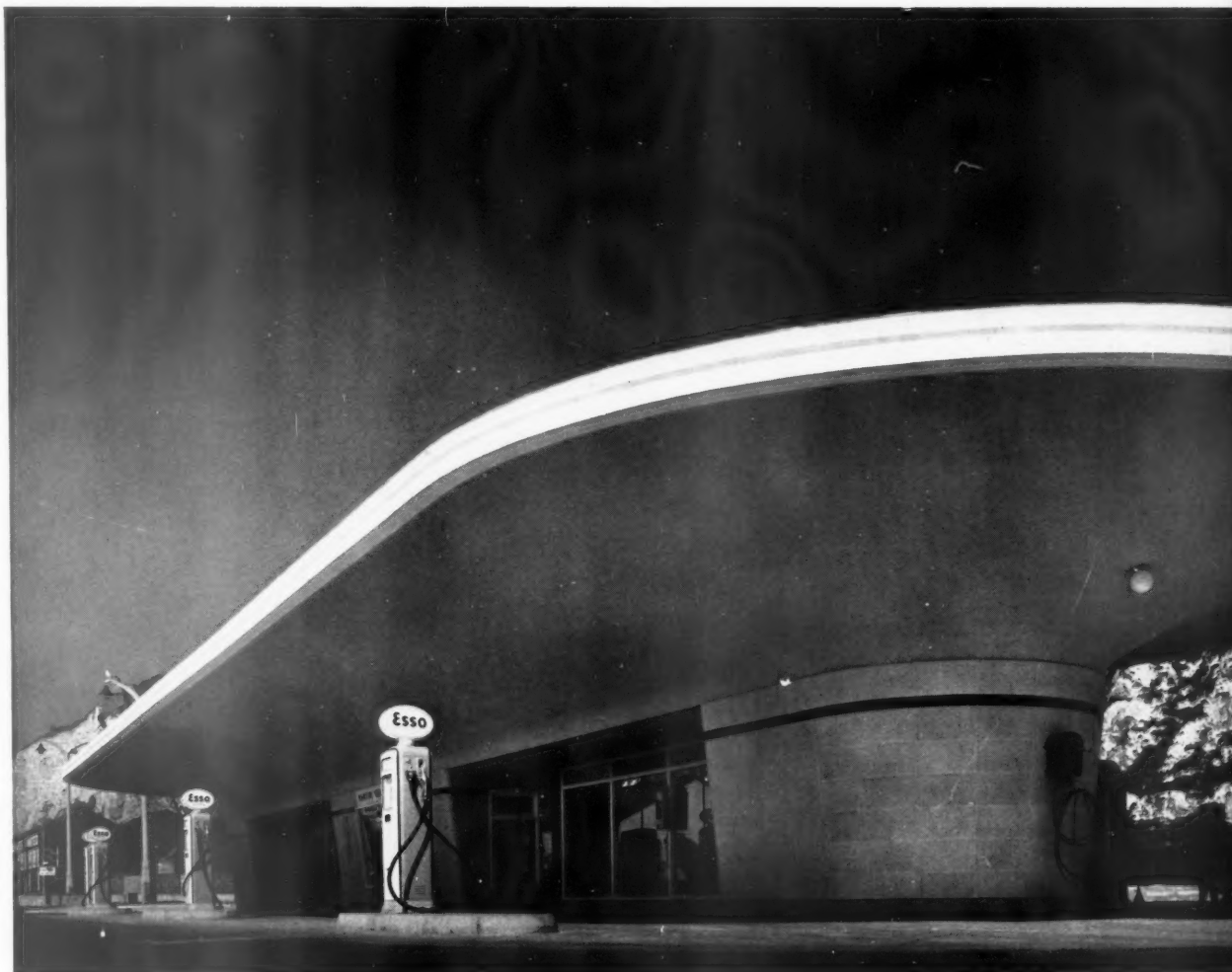
PLAN OF WINDOW. scale  $\frac{1}{4}$  full size

## WORKING DETAIL

ROOFS AND CEILINGS : 12

ROOF: PETROL FILLING STATION AT DOVER

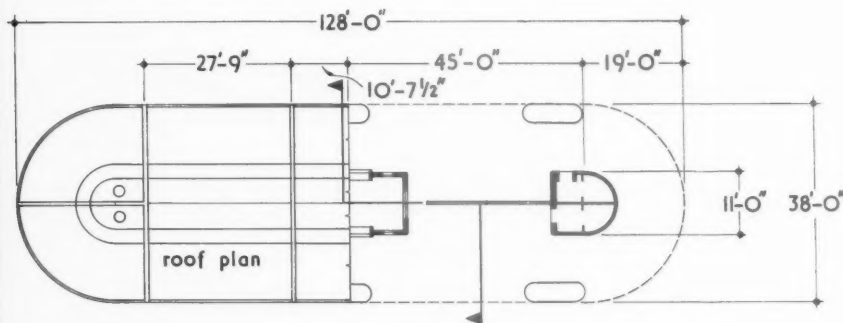
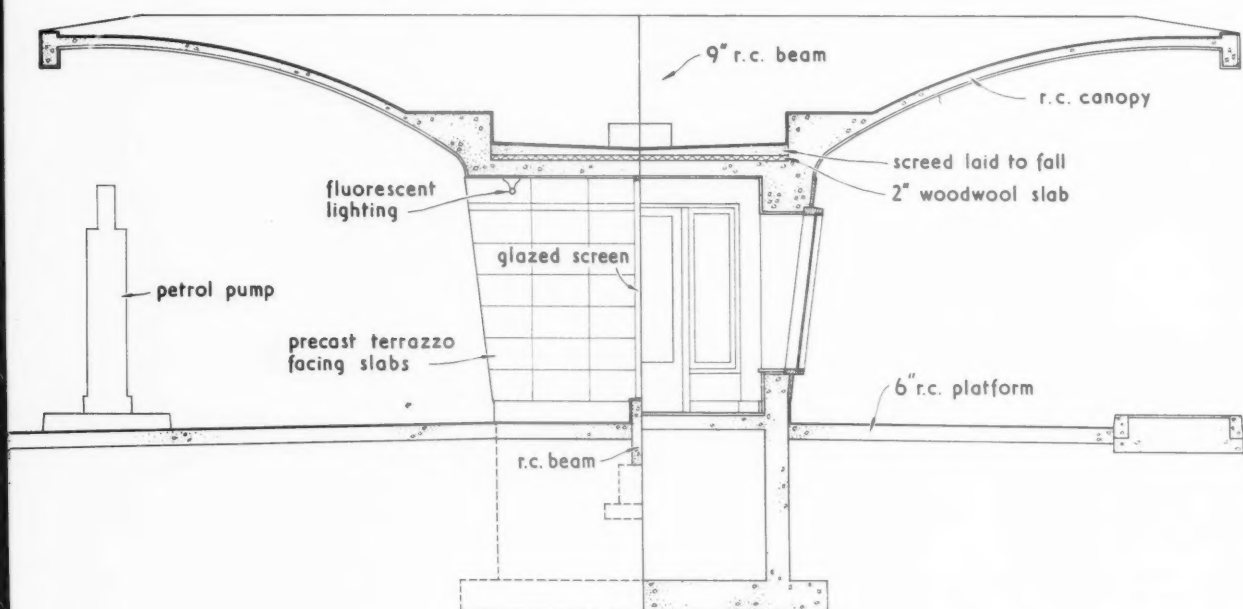
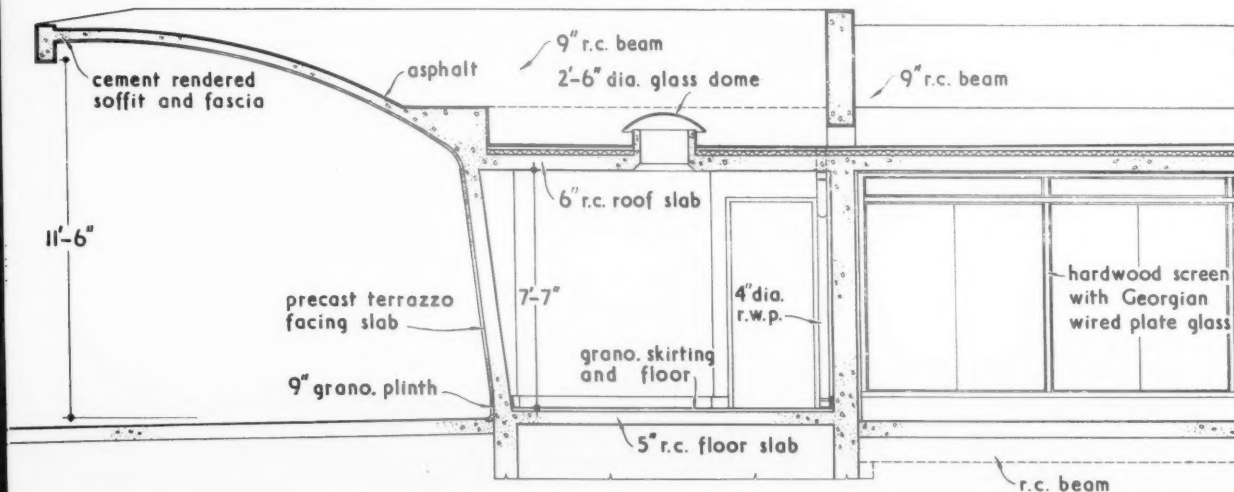
*J. M. Wilson, H. C. Mason and Partners, architects*



*The curved concrete roof overhanging the petrol pumps is supported by cantilevered beams above the roof surface.*

## WORKING DETAIL

ROOF: PETROL FILLING STATION AT DOVER

*J. M. Wilson, H. C. Mason and Partners, architects*KEY PLAN scale  $\frac{1}{32}'' = 1'-0''$ CROSS SECTION. scale  $\frac{3}{16}'' = 1'-0''$ LONGITUDINAL PART SECTION. scale  $\frac{3}{16}'' = 1'-0''$



## WORKING DETAIL

BALCONIES : 7

BALCONIES: FLATS IN LONDON, S.W.10

*Edward Armstrong and Frederick MacManus, architects ; D. J. Tricker, assistant-in-charge*



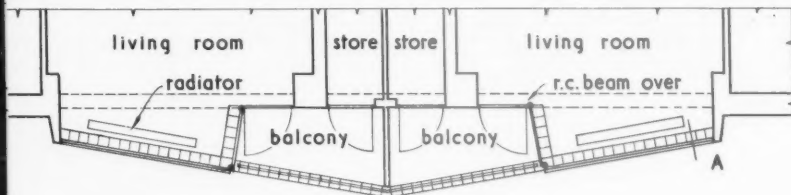
*The facing to the balcony walls is of matt-glazed tiles with a waterproof pointing in the wide joints.*

# WORKING DETAIL

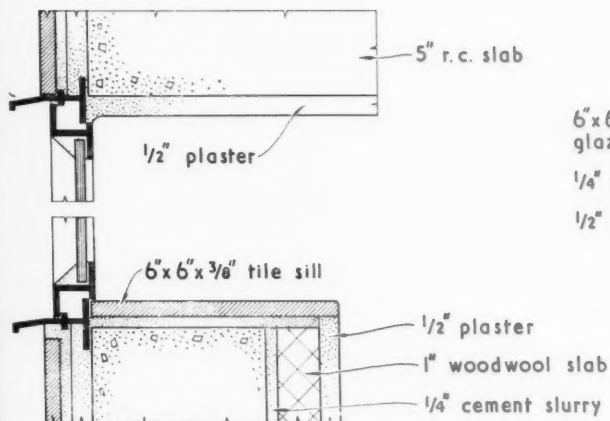
BALCONIES: FLATS IN LONDON, S.W.10

Edward Armstrong and Frederick MacManus, architects; D. J. Tricker, assistant-in-charge

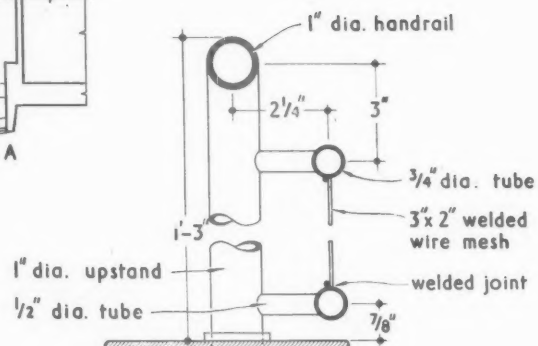
BALCONIES: 7



KEY PLAN. scale  $\frac{1}{8}'' = 1'-0''$

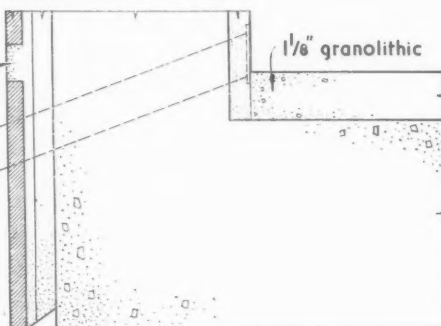


SECTION THRO' WINDOW AT A.  
scale  $\frac{1}{4}$  full size

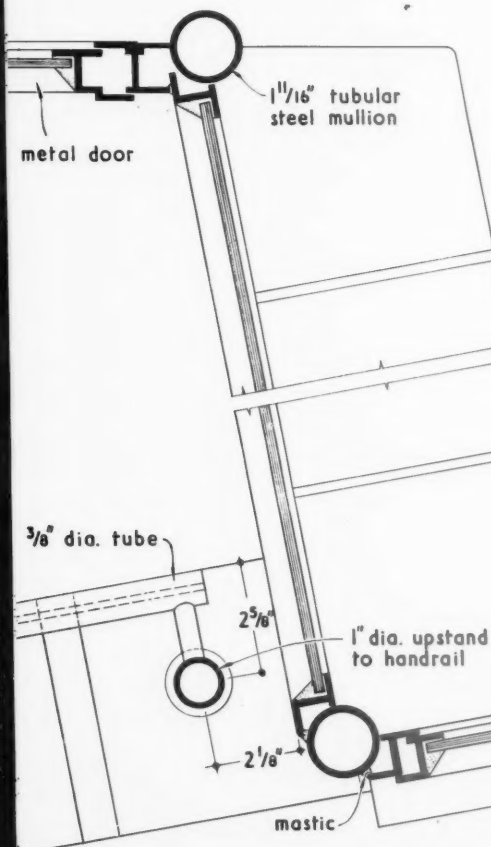


waterproof pointing

r. w. outlet:  
1" dia. copper pipe



SECTION THRO' BALCONY.  
scale  $\frac{1}{4}$  full size



PLAN OF WINDOW. scale  $\frac{1}{4}$  full size

6" x 6" x 3/8" matt-glazed  
tile sill

1/2" plaster

metal window

r.c. surround







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Contractors: R. G. Carter Ltd., Drayton, Norwich.

**F**OR the Diss Secondary Modern School, "PUDLO" Brand Cement Waterproofing Powder was once again selected. In this new building, 'Pudlo' was used in reinforced concrete slabs on the ground floor, and for concrete and brick ducts below floor level. Where the ground rises above floor level, the cavity of the 11" wall has been filled with concrete waterproofed with 'PUDLO.'



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## TECHNICAL SECTION

We mentioned last week how the use of a mobile tower crane on a housing site at Norwich had reduced man-hour requirements by approximately 1,000 per house. Taking into account the cost of the crane, this represents a saving of roughly £100 per house.

This is not a decisive amount—it is not sufficient to ensure that, when a firm that uses a crane competes for a contract with firms without cranes, the firm with the crane will produce the lowest tender.

This figure of £100 demonstrates what we have said many times in the past—that while wages in the building industry remain low (compared with other industries and other countries) there is insufficient incentive for contractors, particularly the smaller firms, to invest in tower cranes and other expensive mechanical aids.

During a period of more-or-less full employment in the building industry, the availability of manpower is one of the main limitations on the amount of building that can take place. Building elements that are in short supply can be replaced by other elements; building labour can only be replaced (or rather, stretched further) by using more mechanical equipment.

We are not concerned here with the justice, or otherwise, of the operatives' demand for an all-round increase of 6d. per hour, but if building labour *did* become an expensive item, as it is in the USA, then the firms that were most highly mechanized would invariably produce the lowest tenders, and the more backward firms would have to follow suit. The increased efficiency of the industry as a whole would counteract the effect on building costs of the increased wage rates, and the capacity of the industry would be increased, with consequent benefit to, amongst others, the architectural profession.

This week's  
special article

### 26 SERVICES & EQUIPMENT: MISCELLANEOUS lifts

The number preceding the week's special article or survey indicates the appropriate subject heading of the Information Centre to which the article or survey belongs. The complete list of these headings is printed from time-to-time. To each survey is appended a list of recently-published and relevant Information Centre items. Further and earlier information can be found by referring to the index published free each year.

*There is, as yet, no standard British lift, and there are surprisingly few regulations concerning the installation of lifts. B. P. Hutton, director of a well-known firm of lift manufacturers, who contributes, below, an article on the subject of installing lifts, strongly recommends the architect to seek expert advice at the earliest possible stage in his design. He recommends as the reference work most useful to architects the relevant BS Code of Practice.\**

Until lift sizes in Britain are standardized, manufacturers will continue to produce purpose-made lifts. This makes it a very simple matter for the architect to induce the lift manufacturer to produce a lift exactly to fit the spaces he has left in his plans, however awkward they may be. (After all, manufacturers are in competition and, now that the

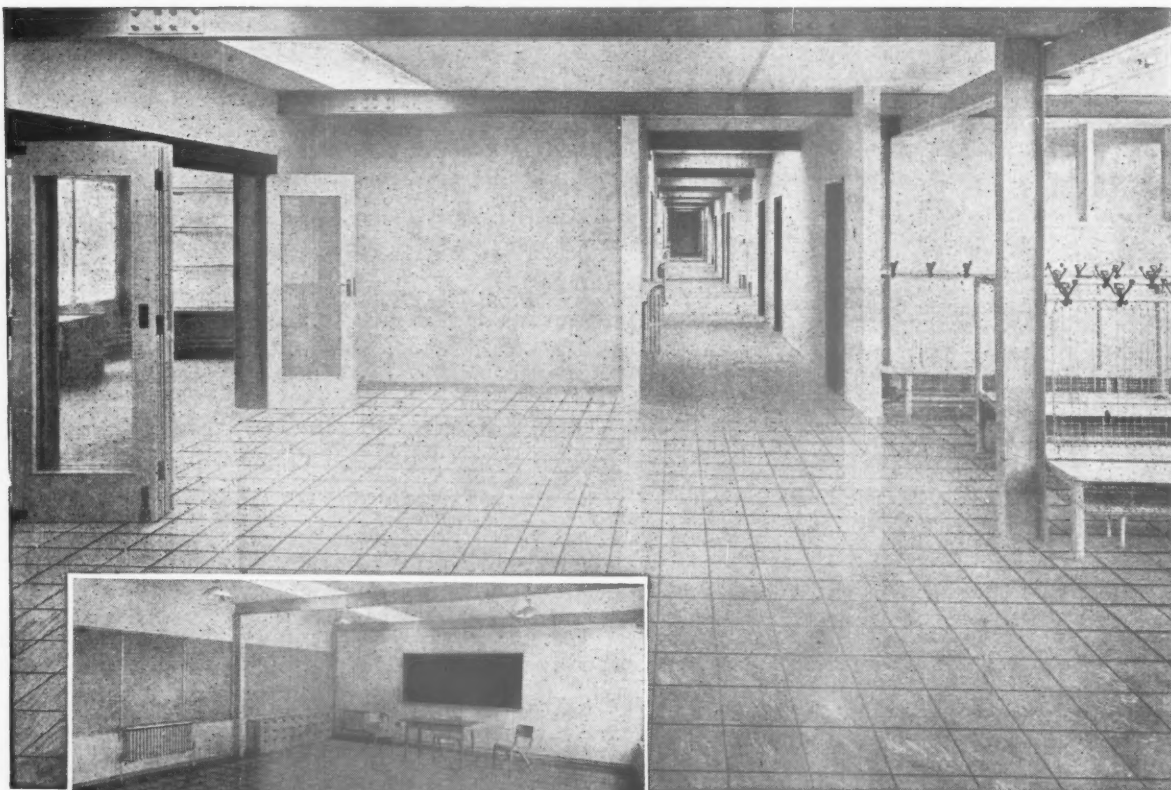
shortage is over, they are all anxious to please.) Nevertheless, there is a good deal the architect should know if he is to avoid making the installation of lifts in his buildings unnecessarily complicated and, hence, unduly expensive.

#### THE MOTOR ROOM

The proper place for the lift motor room is immediately *above* the lift-well,

\* CP 407.101 (1951) (British Standards Institution. 4s.)

## THE LOW-COST FLOOR WITH THE LUXURY LOOK



ACCOTILE FLOORING in the New Hardie County Primary School, West Ham—craftsman-laid by NEUCHATEL in all classrooms, as well as staff rooms, dining room, entrance halls, cloak-rooms, corridors, etc.—approximate area, 900 square yards. Main floors, with contrasting borders, in monochrome patterns:—D.417 Coral Beige; D.414 Carnelian Red; D.464 Sage Grey; D.416 Harvest Gold. Architect and Planning Officer to the County Borough of West Ham: Thomas E. North, Esq., O.B.E., F.R.I.B.A., Dis.T.P. Contractors: West Ham Corporation, Works Department.

# ACCOTILE FLOORING

CRAFTSMAN-LAID BY **NEUCHATEL**

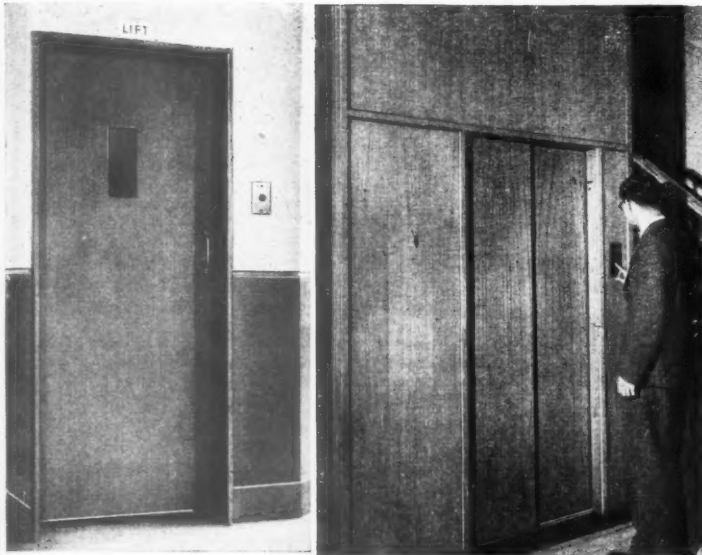
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Left, single-leaf (flush) sliding door: manually operated. N.B. steel lining and architrave. Right, two-speed sliding door, one leaf running at twice the speed of the other, thereby giving full-width entrance in minimum space: automatic.

and the total headroom usually required for the motor room of a passenger lift is not less than 20 ft. from the floor level of the top floor served by the lift. This height requirement presents the architect with a difficult problem, but it is a problem which should not be ignored. Installing the lift machinery at the foot of the lift-shaft increases installation costs, imposes greater loads on the structure, and increases the cost of maintaining the lift.

The inside of the motor room is seldom seen by anyone except the maintenance men (it is normally required that it be

kept locked), but a generous and tidy layout will save money in the long run. Incidentally, there is nothing to prevent your having an observation window, through which visitors to the building can observe the operation of the equipment.

#### LIFT-WELL

The method of constructing the lift-well is often laid down in local regulations, and it is usually required that it should be of fire-resisting materials. Probably the best material is 9-in. brickwork, mainly because of the ease with

which fixings can be made to it, by using tipped electric drills.

Most lift manufacturers now use "T" section guide rails and, having fixed these to the wall, they fix all switches and other fittings to the guides.

Partitions, or reinforced concrete work less than 6 in. thick, are not suitable for fixing lift guides to, and, if fixings can be made only to the floors, installation costs will be increased, since heavier section guide rails will be needed to cope with the span between the fixings.

It is most important that the depth of the pit should be adequate; this varies with the type of lift to be installed and the lift manufacturer should be consulted at an early stage.

Wire grill enclosures are still often permitted when lifts are installed in existing staircase wells, although a solid, fire-resistant enclosure is obviously better. Wire enclosures should always be continuous; they should be fitted inside the stair strings; and they can be used to carry the supports for the guide rails. The other lift gear is usually carried by the structure of the building.

#### ENTRANCES

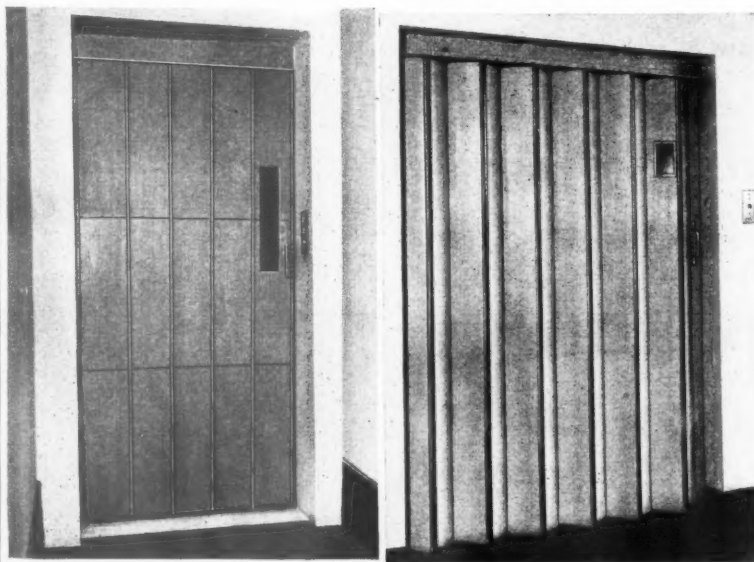
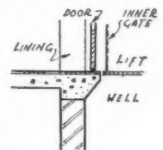
It is usually required that the entrances to totally-enclosed lift-wells should have fire-resistant lift doors, but this is a vague term which is not always properly defined. Most solid lift doors are accepted as providing adequate protection in solid lift enclosures, but sometimes special precautions are required and the fire-resistance of the doors must comply with specified standards.

The best form of protection for lift entrances is the door with a sliding motion. This may be of the "flush," "multi-leaf," or "shutter" type (see illustrations). (The hinged door has practically disappeared from use.)

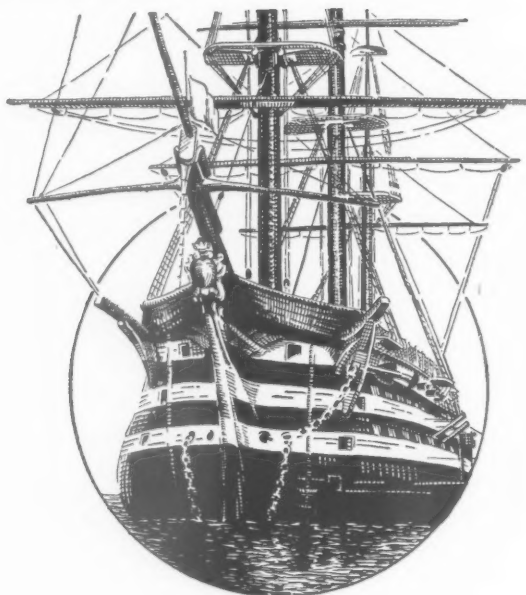
When sliding doors are used, a cill is required, projecting into the lift shaft. (It is usual to project the actual landing into the shaft, as in the sketch, right.) A steel frame is required, from which the doors can be hung; this should not be confused with the lining and architrave (also of metal) often provided in addition.

Concrete is unsuitable as a nosing for lift entrances. For goods lifts, an ordinary mild steel angle will give adequate protection to the arris of the concrete. For passenger lifts, some form of "non-slip" nosing is desirable. This may be supplied by the lift manufacturer and can be combined with the bottom track for the doors, as in the sketch, right.

It is usual to arrange to have the entrance to a passenger lift on the same



Left, multi-leaf (telescopic) door, suitable for passenger lifts where space is restricted and wide opening required. Right, "collapsible" steel shutter gates (sliding/folding) suitable for goods lifts.



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side on each floor, and this should be done wherever possible. Hospital lifts should have the entrance on the *narrow* side of the lift. It is often necessary to provide two entrances to goods lifts, but these should always be on opposite sides of the lift, not on adjacent sides. Lift manufacturers detest what they call the "cross-corner" car. They cannot place the guides at the side with the entrance, so they have to go in the corners; the corners have to be cut off the car in order to accommodate the guides; and the result is untidy and uneconomical.

#### SPEEDS

Careful thought should be given to the question: at what speed should the lift run? The speed of the lift will affect considerably both installation and running costs. The speed chosen should be the lowest speed which will provide an adequate service. Always remember that the time taken up in loading and unloading often exceeds the time the lift takes to get from floor to floor, so that an increase in speed does not effect a *proportional* decrease in operating time.

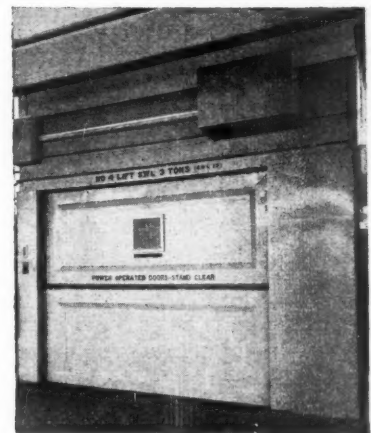
For lifts serving only two or three floors, a speed of 50 to 60 ft. per min. is usually fast enough. The maximum speed for a single-speed lift is 100 to 120 ft. per min.; above this speed it is necessary to introduce additional equipment for slow running when the lift approaches floor level, in order to give reasonably accurate "levelling." Above about 300 ft. per min. the equipment becomes even more complicated (a motor-generator is required to give variable voltage control), and for speeds between 400 and 600 ft. per min. expensive equipment with gearless drive is needed.

#### MAINTENANCE

A lift cannot be taken away for repairs like an automobile; hence, if it is to be



Vertically "bi-parting" (sliding) steel shutters: power operated: suitable for heavy-duty goods lifts.



kept in a safe and efficient condition, service engineers must visit the building at regular intervals. If the building is in a remote area, the frequency of these visits should be minimized by choosing the simplest type of equipment—a single-speed lift; manually-operated gates; straightforward controls (no equipment for collecting passengers *en route*), etc.

Before designing a building overseas that will need a lift, make sure that lift manufacturers can provide maintenance service in the area.

#### SERVICE LIFTS

Lifts which do not carry passengers are designed to different standards from passenger-carrying lifts and must, therefore, be limited in size. They usually carry a load of about 112 lb., but they can be designed to carry much heavier loads when necessary.

The opening to the lift is usually about 2 ft. 9 in. above floor level, but some-

times it is at floor level. Doors or shutters must be fire-resistant, as with ordinary lifts.

Service lifts which open only at serving board height are often not provided with the electrical system which prevents the doors from being opened when the lift is not present (there being less danger of falling into the shaft than when the opening is at floor level), although electrical contacts are always provided so that, if the lift is in motion when the doors are opened, it is brought to a stop. However, the absence of automatic locks may mean that the lift installation contravenes local fire regulations, and these should be studied before the details of the lift specification are finalized.

#### ENQUIRY FORM

I am interested in the following advertisements appearing in this issue of "The Architects' Journal." (BLOCK LETTERS, and list in alphabetical order of manufacturers' names please).

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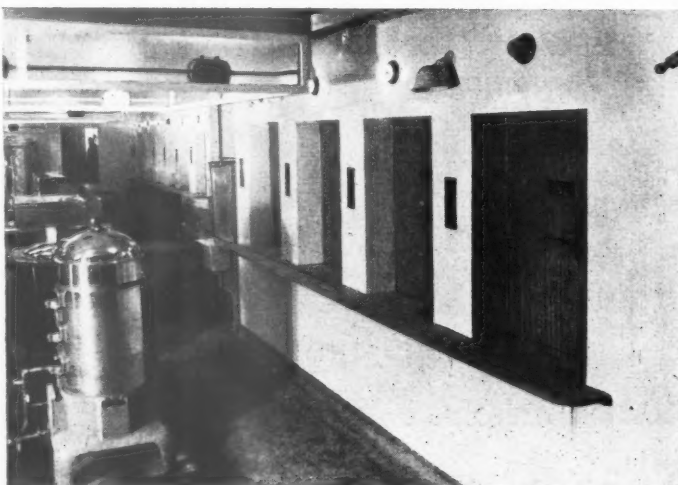
Please ask manufacturers to send further particulars to:—

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PROFESSION or TRADE .....

ADDRESS .....

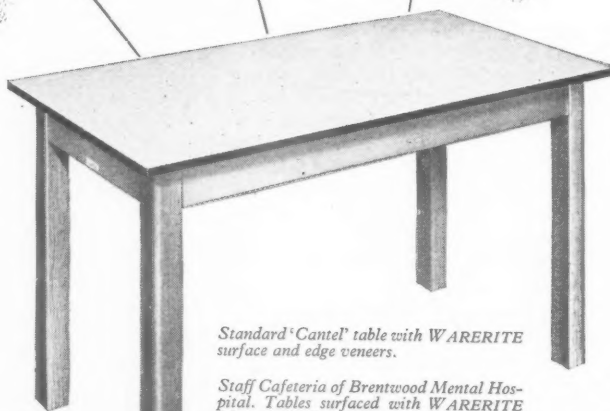
AJ 10953



Bank of service lifts, with steel shutters, at the Regatta Restaurant, Festival of Britain (architects, Misha Black and Alexander Gibson). Note the arrival indicators to each lift.

# Five thousand finger exercise

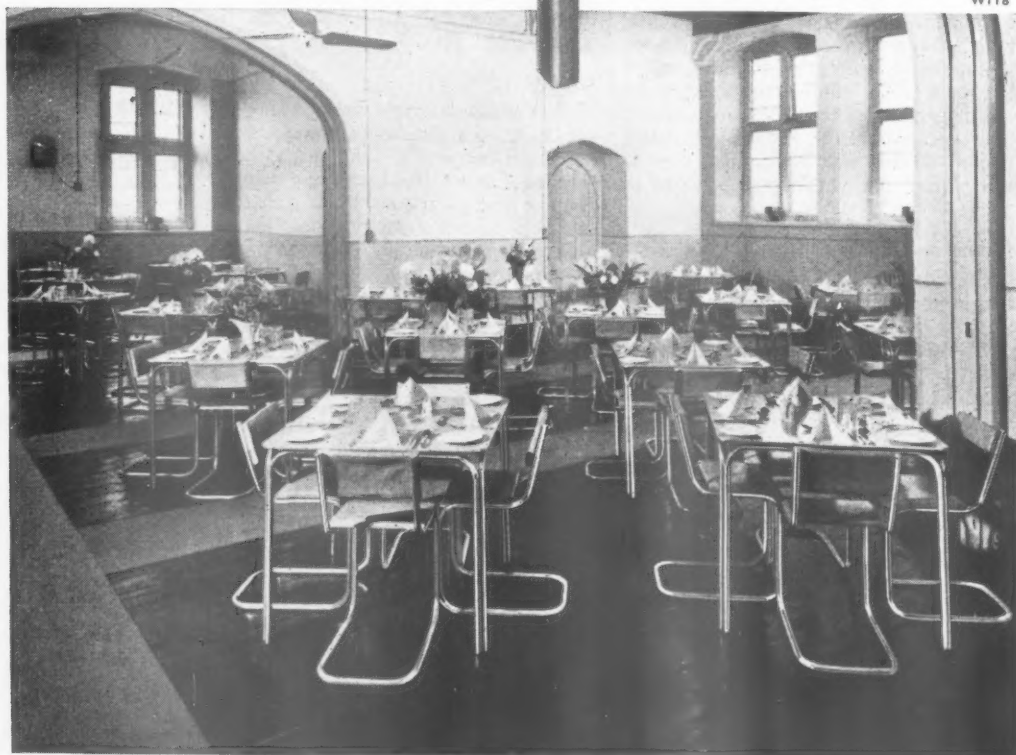
Fingers and forks, pots and plates, the continual spilling of food and drink give canteen table top and counter more than their fair share of drudgery. But where WARERITE Surfaces are used to meet this onslaught, even the most delicate patterns are safe from harm. WARERITE Plastics are hard wearing, hygienic, resistant to acids, alkalis—and to burning cigarettes, if required. There are 'Weaves' and 'Woodprints', and colours or patterns to order. Specify them for all cafeteria furnishing, including wall surfaces.



Standard 'Cantel' table with WARERITE surface and edge veneers.

Staff Cafeteria of Brentwood Mental Hospital. Tables surfaced with WARERITE Material in Birds' Eye Maple pattern.

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## THE INDUSTRY

*From the Industry this week, Brian Grant reports on a bath designed to use less hot water, a new type of steel floor and four useful booklets from manufacturers—one on time control, one on concrete reinforcement, one on drain fittings and one on kitchen planning.*

## COPPER PRICES

The re-starting of the free market in copper is a good thing for the building industry in that it has helped to bring the price down by £30 or so per ton. Even before this price drop, copper was quite a competitively priced material for plumbing work and, although the amount used per house is only to be measured in hundredweights, anything which tends to reduce costs is to be encouraged.

## TIME CONTROL

A useful booklet recently issued by Blick Time Recorders deals with electrical and mechanical time recording systems, mains- and battery-driven clocks, staff signalling systems and the various other services now needed in offices and factories. A handy publication which gives a clear idea of what can be done with modern equipment. (Blick Time Recorders Ltd., 96-100 Aldersgate Street, London, E.C.1.)

## PAINT DISPLAYS

The new paint display at the Building Centre, by Goodlass, Wall & Co., seems to me to represent a sensible approach to the problem of painting. It starts with a series of panels showing what to avoid, continues with examples of appropriate primers for different materials, and ends with notes on different types of paint for finishing coats. This is very much better than the usual ultra high gloss panels all looking so beautiful that one ends up with the idea that everyone's paint is as good as everyone else's. (Goodlass, Wall & Co. Ltd., 179-185, Great Portland Street, London, W.1.)

## A NEW SHAPE OF BATH

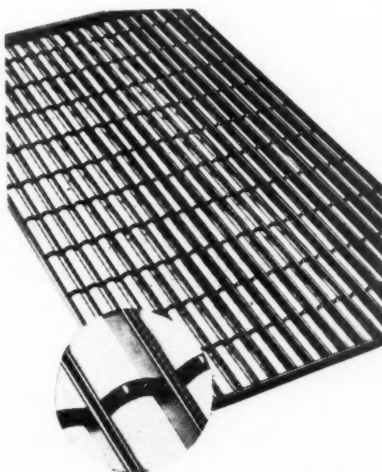
The drawing below shows a new shape of bath, which, while costing no more than standard types, saves about one-third of the water normally used. It is made in 5 ft. 6 in., and 6 ft. sizes, the internal dimensions being 4 ft. 11½ in. and 5 ft. 5½ in. respectively. To some people, the vaguely coffin-like shape may be a little depressing, but it is logical enough, as it provides a greater width at shoulder level than the

standard bath, and the reduction in width at the foot should not be found inconvenient.

The height from the floor to the roll is only 17 in., which should make the bath easier to clean and children easier to wash. The manufacturers also suggest that it should be easier for invalids or elderly people to get in and out of. There is the minor disadvantage that there is less than usual of the narrow edge on which soap racks can be slung but, on the other hand, there is more room on which to sit. The bath is known as the "Doune" and the standard colours are white, green, cream and blue. (Jackson, Elphick & Co. Ltd., Larkhall, Scotland.)

## STEEL FLOOR PANELS

There are many industrial buildings in which a safety steel floor is required—around machines, for the treads of stairs or for overhead cat walks. A new type has just been announced by Chamberlain Industries and is known as the "Rodlock." The panels can be supplied in any length up to 18 ft. and, while the standard width is 2 ft., it can be any multiple of 1½ in. The flooring is made of mild steel bars, ⅝ in. thick, and in three depths, from 1 in. to 1½ in., according to the loading required and the spans between supports. The bars are connected by ½-in. rods at 4½-in. centres; the rods, after being threaded through holes in the bars, are pressed into a U-shape, the top of the U being level with the top of the bars. The panels can be cut to fit round stanchions or other obstacles, and margin bars are usually welded round the cut areas to provide a clean finish. (Chamberlain Industries Ltd., Staffa Works, Leyton, London, E. 10.)



Above, "Rodlock" steel safety flooring, with close-up inset. Right, part of the 70-ft. long service counter in the canteen for Leyland Motors, installed by James Stott & Co.

## CONCRETE REINFORCEMENT

McCall's have recently issued the 4th edition of their concrete reinforcement handbook, containing wire and bar details, notes and design data on the use of "Matobar" welded mesh reinforcement, and a further section on "Macalloy" steel bars for prestressing on the Lee-McCall system. This firm also manufactures, under licence, the "Isteg" twin-twisted reinforcing bar, in which two separate mild steel bars are twisted together between fixed centres, the cold work increasing the yield stress so that the working stress may be increased by 50 per cent. over that allowed with plain bars. (McCall & Co. (Sheffield) Ltd., Templeborough, Sheffield.)

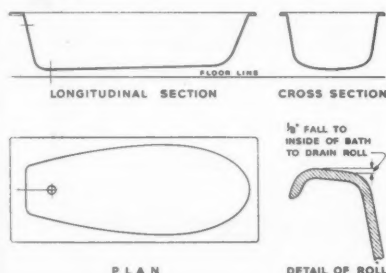
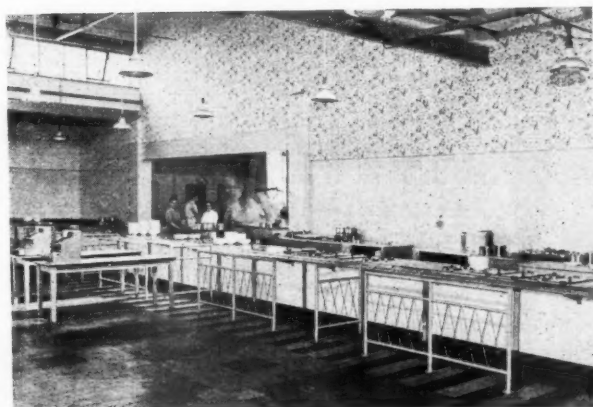
## DRAINAGE SPECIALITIES

A new catalogue from the Leeds Fireclay Co. gives full descriptions of their salt-glazed drain pipes and fittings, all of which are manufactured to BS65. Pipes, fittings, traps, junctions, interceptors and gullies of all kinds are illustrated, not forgetting the necessary ironwork. A useful booklet. (The Leeds Fireclay Co. Ltd., Wortley, Leeds, 12.)

## PLANNING INDUSTRIAL KITCHENS

From Stotts, of Oldham, comes a very handy booklet which contains much useful data on the planning of large-scale kitchens for industrial catering. In the booklet is described an imaginary kitchen of 1,200 sq. ft., which would cater adequately for 250 people. The planning is explained step by step, from cold store, via vegetable and general stores, to offices and lavatories, preparation and cooking areas and, finally, the service counter or hot cupboard and the washing-up area. The reasoning on which the placing of the various items is based seems sound, and there are several pages of general data, including an excellent table showing the numbers and sizes of the different types of machine—potato peelers, chippers and so on, needed when various numbers of meals are to be served.

The illustration below shows the kitchen of a canteen installed by Messrs. Stott for Leyland Motors. It is designed to provide meals for 1,500 people and there are a number of ideas which might well be copied in other factories. One section of the canteen, for instance, is set aside for those who wish to bring their own food; it contains two water boilers, so that employees can make their own tea, and a sink where they can wash up their own crockery. A further section of the canteen contains a snack counter, 30 ft. long. This being quite distinct from the main service counter at which complete "plated" meals are served. This is 70 ft. long, with steam-heated cupboards and soup containers. It will serve as many as 1,000 meals in ten minutes. (James Stott & Co. (Engineers) Ltd., Vernon Works, Oldham, Lancs.)



## Buildings Illustrated

*Houses, Flats and Old People's Bungalows, Longmead Road, Weston Green, Esher, Surrey, for Esher UDC.* (Pages 318-322.) Architect: Eric Lyons, F.R.I.B.A., M.S.I.A.; assistant-in-charge: Geoffrey Scoble. Consultant Engineer: Z. Pick, A.M.I.C.E., A.M.I.S.T.R.U.C.T.E., M.S.O.C.E.(FRANCE). Quantity surveyors: Thomas Barrett, Sons & Partners. General contractor: H. B. Kingston Ltd. General foreman: L. A. French. Sub-contractors: dampcourses, The Ruberoid Co. Ltd.; concrete blocks, Wadcrete Ltd. (houses only); reinforced concrete, Smiths Fireproof Floors Ltd. (flats and bungalows only); The Square Grip Reinforcement Co. (Sunbury) Ltd. (bungalows only); bricks, Marston Valley Brick Co.; frostproof tiles, supplied by Carter & Co. (London) Ltd. (flats and bungalows); copper roofing, special roofings, Hugh Twaddle; roofing felt, William Briggs & Sons Ltd. (flats and bungalows); mastic asphalt flooring, Highways Construction Ltd.; back-boilers and grates, Newton Chambers Ltd.; electric wiring, Langston Jones Ltd.; electric light fixtures, The Merchant Adventurers of London Ltd. (flats and bungalows); Simplex Ltd. (flats); sanitary fittings, Fred Hodge Ltd.; door furniture, Stedall & Co. Ltd.; wood casements, H. Newsum, Sons & Co. Ltd., Boulton & Paul Ltd. (flats and bungalows); metal casements, Gardiner, Sons & Co. Ltd. (flats); revolving doors, S. G. Day (flats); distemper and paints, ICI Ltd., Silixine Ltd. (houses), George Lillington & Co. (houses); metalwork, S. G. Day (flats and houses); kitchen fittings, Built-in Fixtures Ltd.; wallpapers, The Wallpaper Manufacturers Ltd. (flats); fireplace surrounds, W. N. Froy & Sons Ltd. (flats); signs (house numerals), The Lettering Centre Ltd.

## Announcements

W. A. E. Sewell, A.R.I.B.A., and Rosemary W. Gold, A.R.I.B.A., are now in practice at 180, Piccadilly, W.1, under the style of Sewell & Gold. (Tel.: Hyde Park 4404.)

Stroud & Nullis, A./A.R.I.B.A., of 25, Ebury Street, S.W.1, have opened a branch office at 3, Church Path, Woking, Surrey, where they will be pleased to receive trade catalogues, etc.

J. R. M. Poole, A.R.I.B.A., has resigned his position as Chief Architect of the Timber Development Association to take up a Government appointment. Since joining the TDA in February, 1950, Mr. Poole has led the team of engineers and architects who constitute the Design and Construction Department.

Cowell, Drewitt and Wheatly, F./F.R.I.B.A., have now opened an office at 1, High Cross Street, St. Austell, Cornwall, where they would be pleased to receive trade catalogues.

Harris & Porter, Chartered Quantity Surveyors, have moved their Portsmouth Offices from 103, Palmerston Road, Southsea, to 148, Commercial Road, Portsmouth. (Tel.: Portsmouth 4221.)

Peter Brunn, M.S.I.A., has moved to 31 Pont Street, S.W.1. (Tel.: Sloane 3115.)

Three representatives of the Arcon Group, W. F. Lutyens, A. M. Gear, A.R.I.B.A., consultant designer, and J. L. Lishman are making a 12,000 mile tour of Canada to investigate possibilities of the Canadian market for factory-made buildings. They arrived in Montreal on September 1. In a 6,000 mile tour by road, rail and air they will visit Vancouver, Ottawa, Toronto, Winnipeg, Calgary and Edmonton.

The governing council of the Fibre Building Board Development Organisation Ltd. has appointed a technical committee to deal

with research, development and the dissemination of information to architects, engineers and designers and to the operatives and technicians concerned in the various fields of utilisation of the material. The information thus gained will be pooled and made available to manufacturers, agents and distributors of fibre building board.

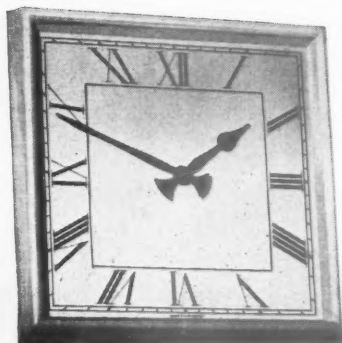
The technical committee consists of seven members, under the chairmanship of E. P. Lawrence, who is also chairman of the governing council of the FBBDO. Other committee members include J. Ballantyne and H. Rixon, both nominated by the Building Board Manufacturers' Association of Great Britain Ltd.; J. P. Blackman and J. G. G. Stone, Fibre Building Board Agents' Association Ltd.; C. S. Goyder and A. W. Shaffery, the Insulation, Building and Hard Board Association Ltd.; and R. V. Cooper, nominated by Overseas Interests.

Mr. Rixon, Mr. Goyder and Mr. Cooper are also members of the FBBDO governing council. The secretary of the technical committee is Basil Marriott, technical director of the FBBDO.

The technical committee has met twice. Among the many subjects for early action is the publication of a new and revised edition of "Fibre Building Boards," the basic work of reference of the fibre building board industry.

Haskins Ltd., makers of Rolador and Interlocking Shutters, and Hammond & Champness, makers of electric and hand power passenger and goods lifts, have opened new offices at 32, Manchester Street, Birmingham, 6. (Tel.: Aston Cross 1456.) Full sales and service facilities are available in Warwickshire, South Staffordshire, Shropshire, Leicestershire and Worcestershire. Illustrated catalogues and detailed information are obtainable from the office manager, J. A. Shaw, or the area representative, E. G. Daniels. (Tel.: Sutton Coldfield 1890.)

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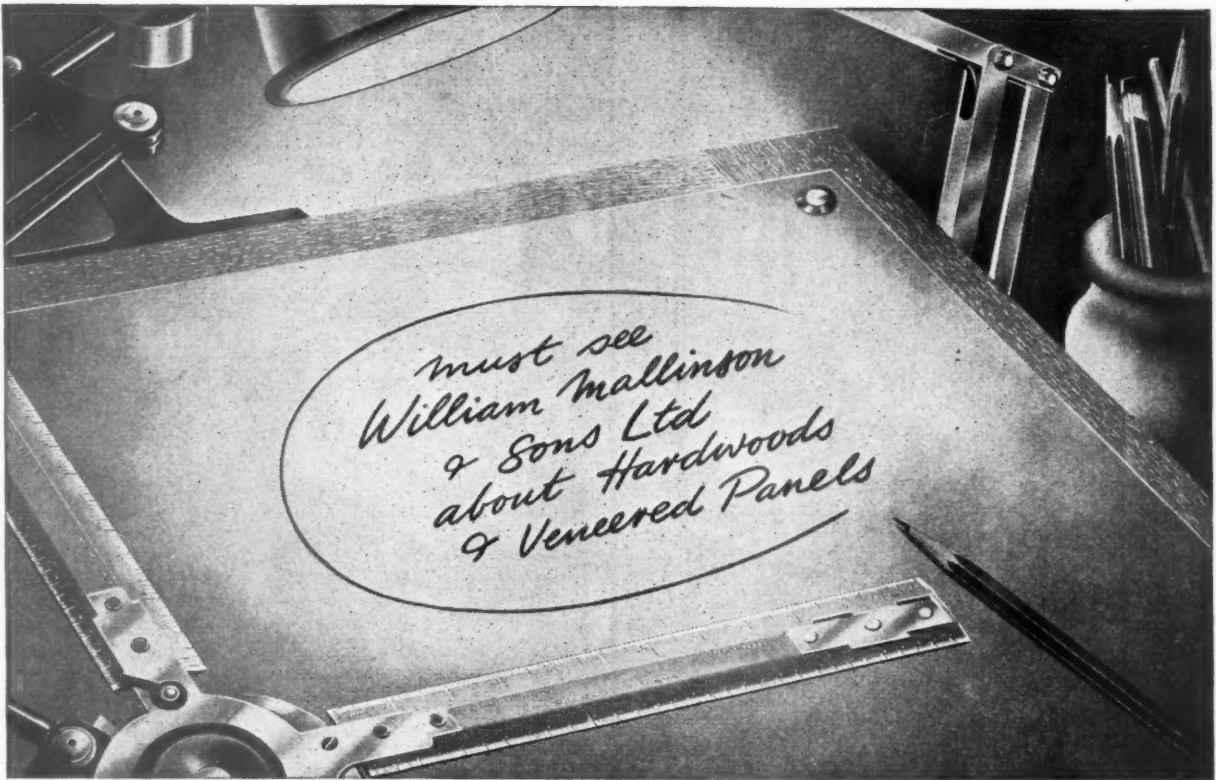
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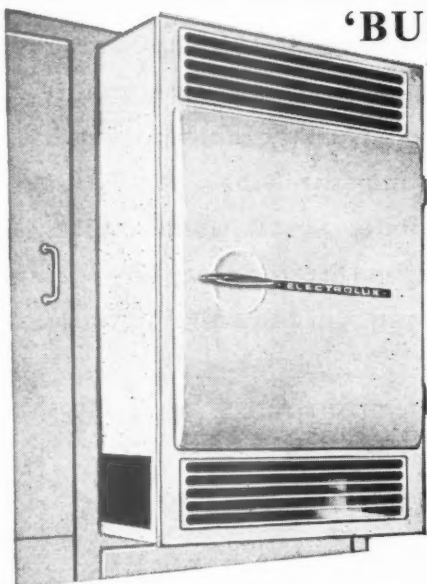
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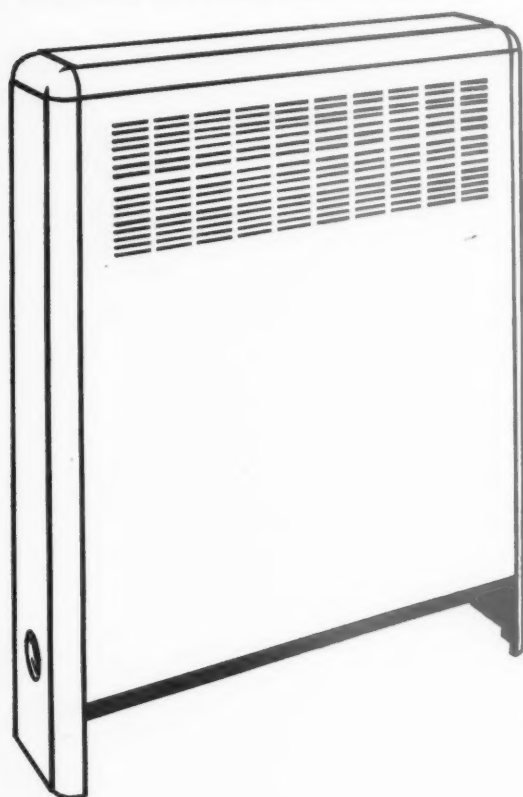
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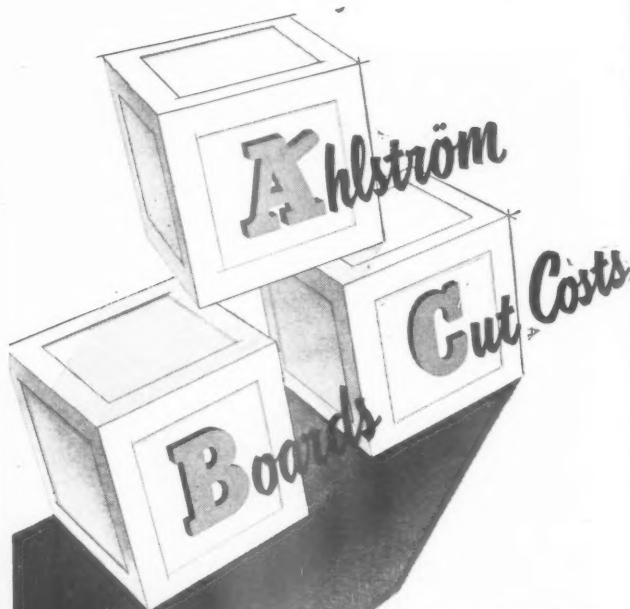
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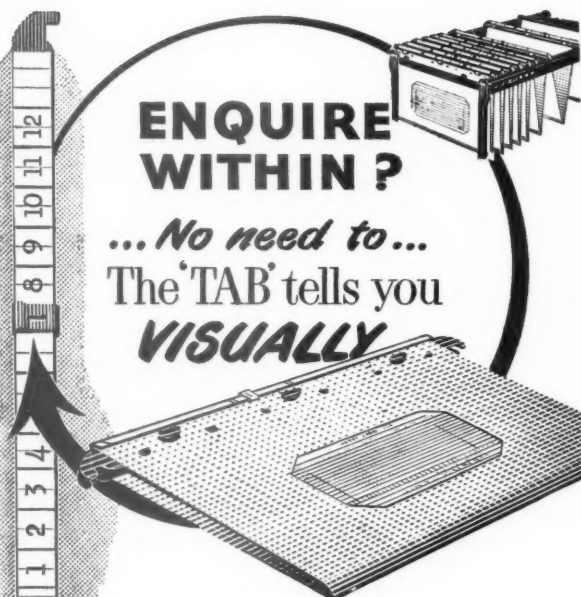
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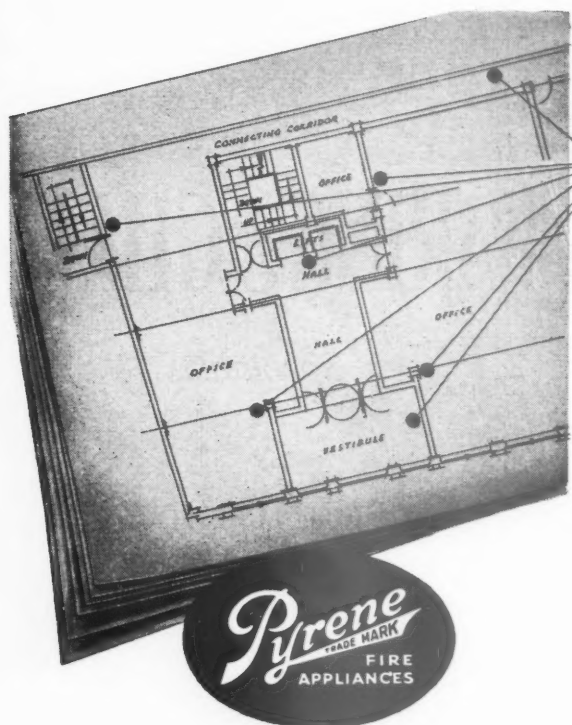
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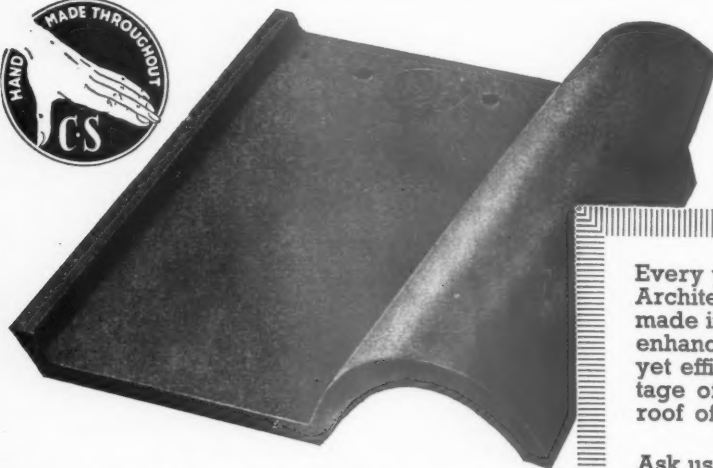


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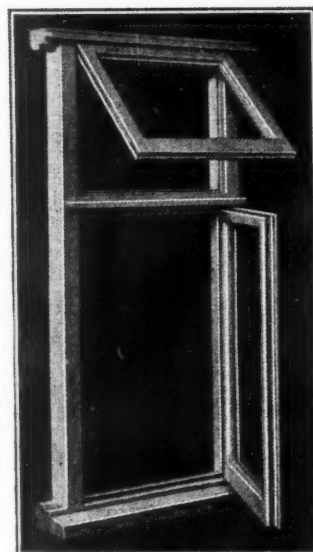
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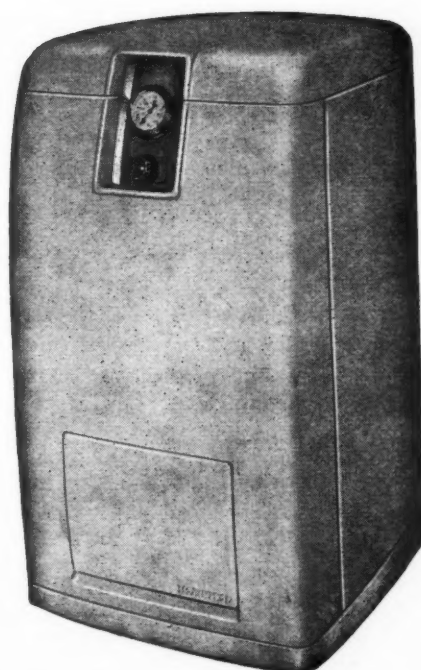
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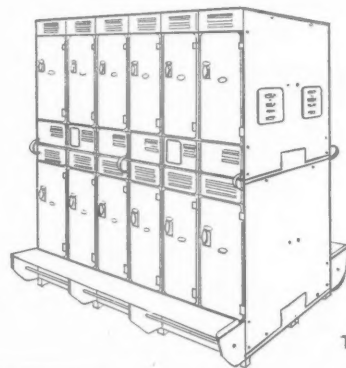
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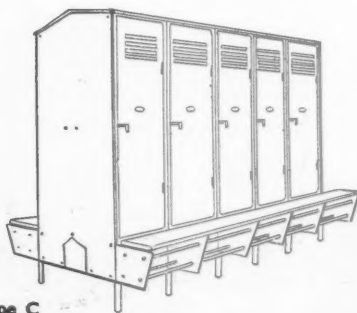
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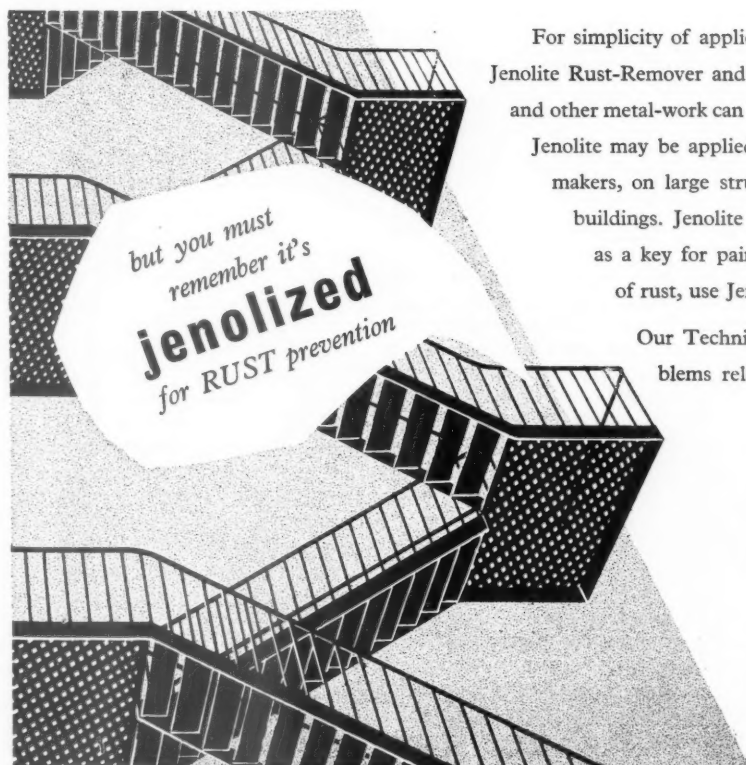
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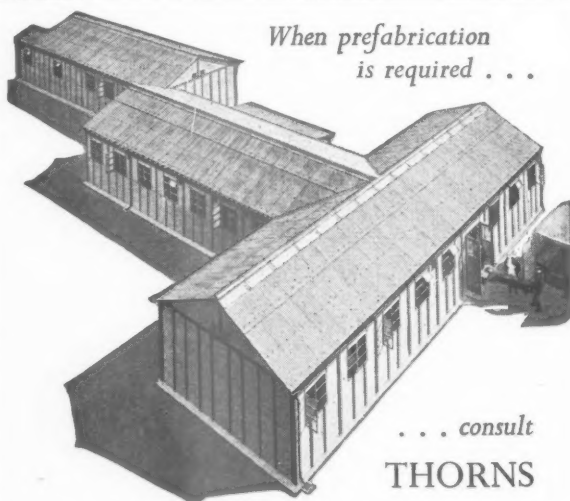
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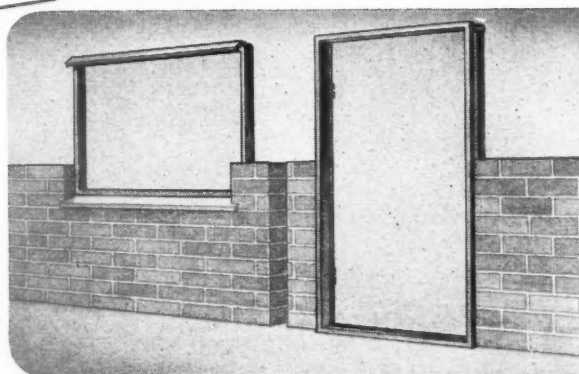
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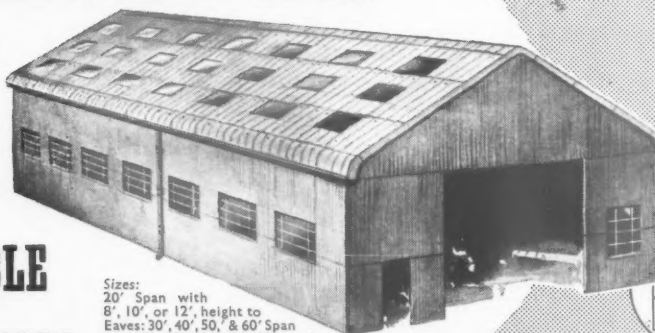
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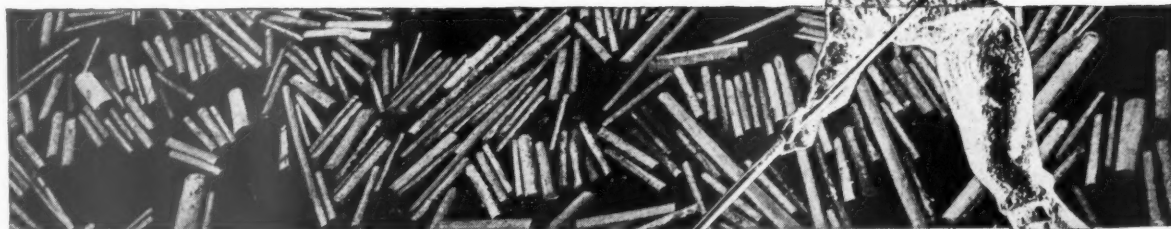
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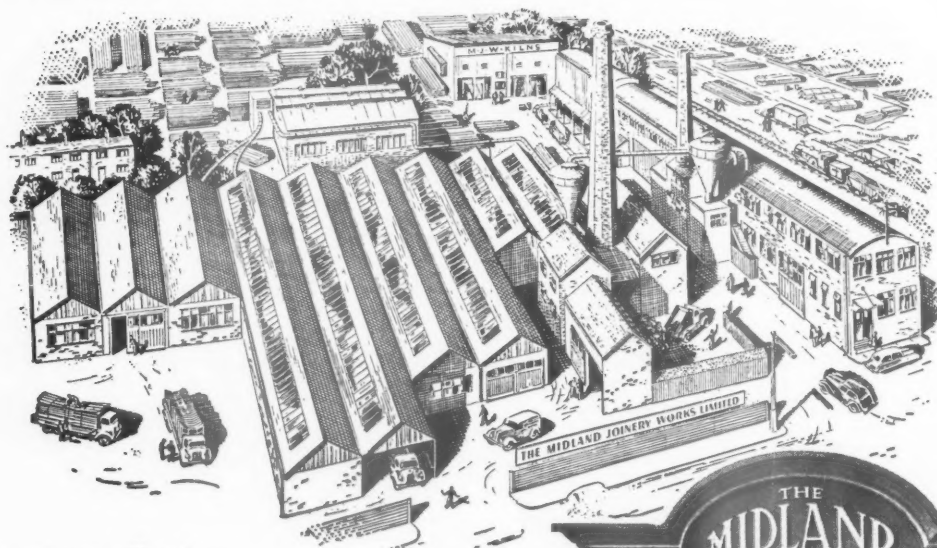
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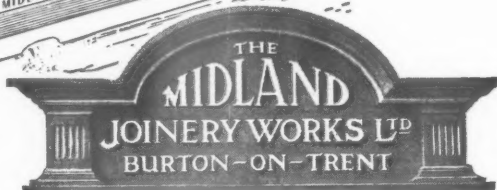
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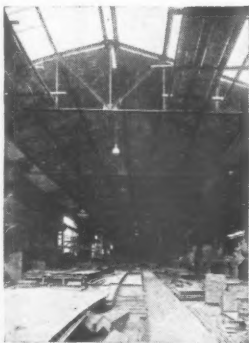
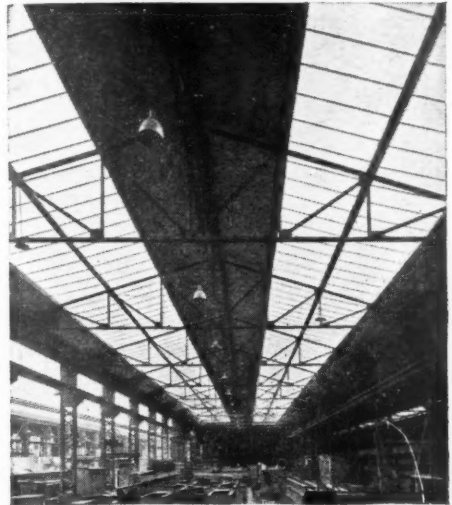
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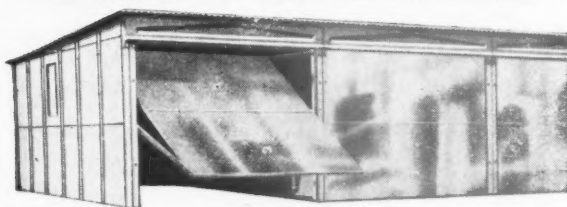
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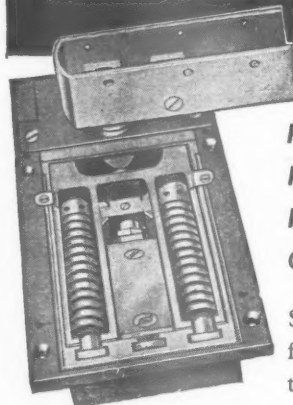
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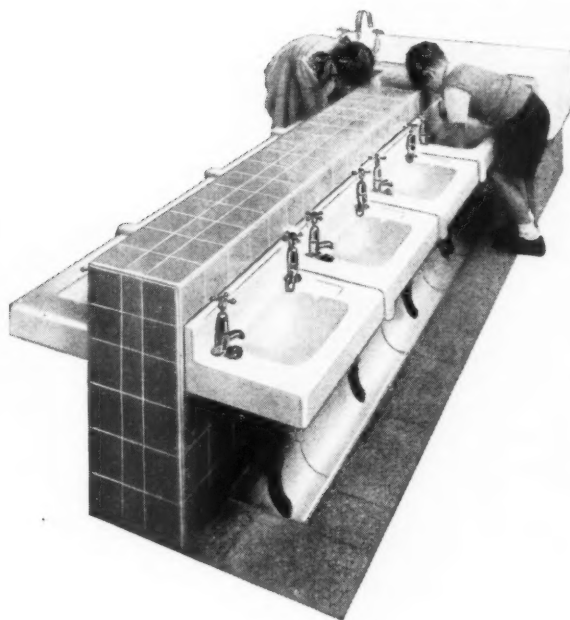
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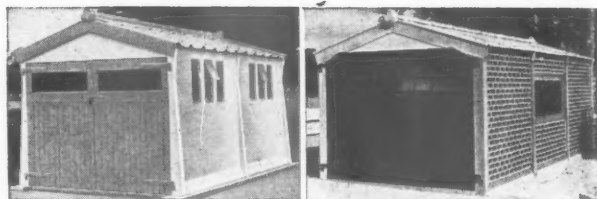
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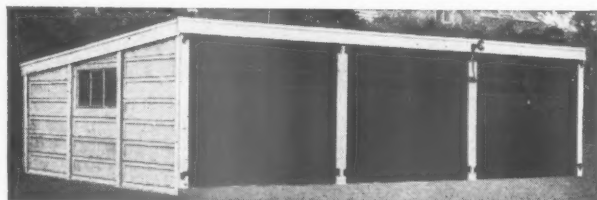


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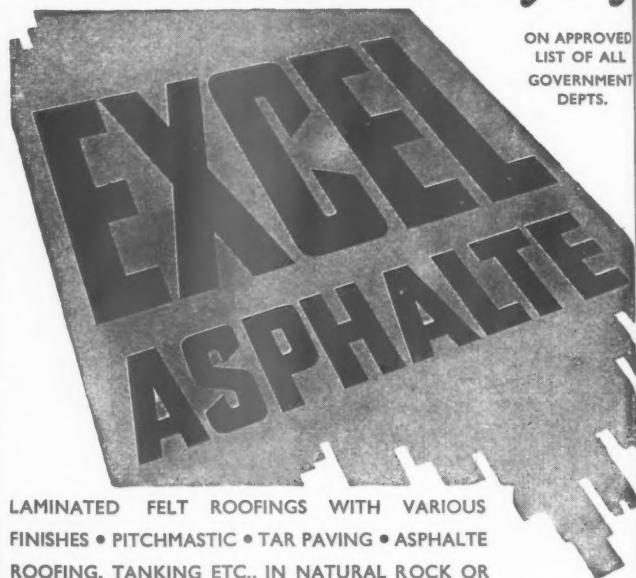
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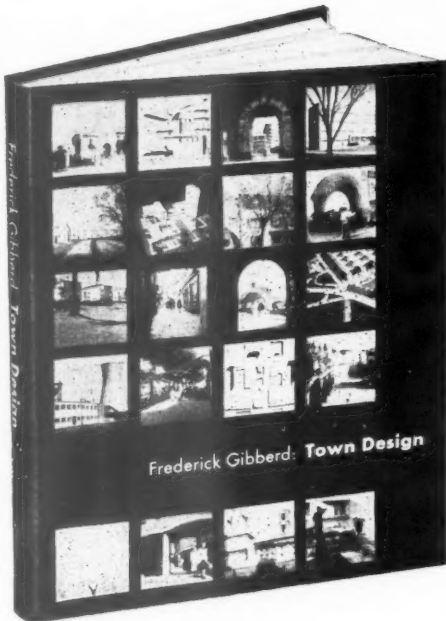
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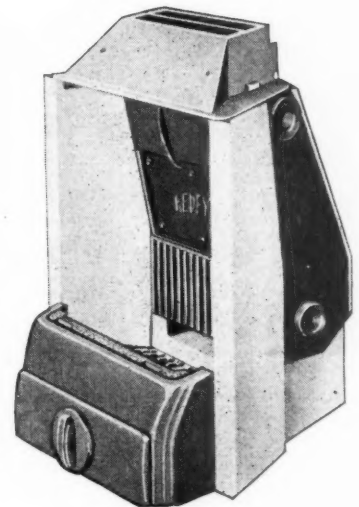
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**REDFYRE**

**No. 4 SUPER  
BACBOILER**

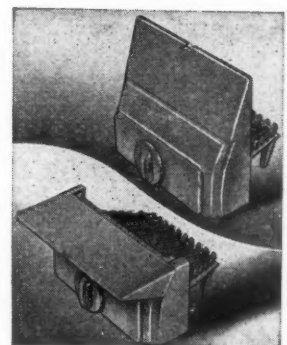
The "Redfyre" No. 4 Bacboiler, providing the warmth and comfort of the open fire is designed to burn coke, coal or anthracite. Its heating capacity ensures abundant domestic hot water, together with the heating of a bathroom towel-rail and two radiators for bed-room, lounge or hall.



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Incorporating the "Redfyre" heat resisting bottom grate, made from 30% chrome semi-steel which eliminates the problem of burnt out fire bars.



**NEWTON CHAMBERS  
& COMPANY LIMITED**

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## CLASSIFIED ADVERTISEMENTS

Advertisements should be addressed to the Advt. Manager, "The Architects' Journal," 9, 11 and 13, Queen Anne's Gate, Westminster, S.W.1. and should reach there by first post on Friday morning for inclusion in the following Thursday's paper.

Replies to Box Numbers should be addressed care of "The Architects' Journal," at the address given above.

## Public and Official Announcements

25s. per inch; each additional line, 2s.

The engagement of persons answering these advertisements must be made through a Local Office of the Ministry of Labour or a Scheduled Employment Agency if the applicant is a man aged 18-64 inclusive or a woman aged 18-59 inclusive unless he or she or the employment, is excepted from the provisions of the Notification of Vacancies Order, 1952.

### CITY OF SALFORD.

Applications are invited for the appointment of Architectural Assistant, Salary A.P.T. Grade IV (£555-£600 per annum).

Candidates must have passed the Intermediate Examination of the R.I.B.A. or its equivalent at one of the recognised Schools of Architecture and have had at least two years' experience in an Architectural Office.

The post is subject to the National Scheme of Conditions of Service, the passing of a Medical Examination, and is superannuable.

Detailed particulars with the names of three referees to be sent to the City Engineer and Surveyor, Town Hall, Salford, endorsed "Architectural Assistant (Ref: A.J.)", so as to be received not later than Monday, 14th September.

Applicants must disclose in writing any known relationship to Members or Officers of the Council.

H. H. TOMSON,  
Town Clerk. 9447

### LANCASHIRE COUNTY COUNCIL.

SECTIONAL PLANNING OFFICER, A.P.T. VIII (£760-£835), required in Accrington Divisional Planning Office. Considerable experience in Town Map preparation and administration of Town and Country Planning Acts and Orders is essential. Candidates should be qualified Architects; a planning qualification in addition would be an advantage. Applications, giving experience, present salary, and two referees, to the County Planning Officer, East Cliff County Offices, Preston, by 19th September, 1953. 9479

### BRITISH ELECTRICITY AUTHORITY. EAST MIDLANDS DIVISION.

Applications are invited for the following positions within the Division:

**CIVIL ENGINEERING DRAUGHTSMEN.** Construction Department. (Vacancy No. 22/53.) Candidates should have experience in design and detail of reinforced concrete structures, piled and slab foundations for heavy plant, culverts, cable subways, etc., for general building construction drainage and sanitation schemes, associated with office and administrative buildings.

The salary will be in accordance with Grade 5 (£567-£671 per annum) or Grade 6 (£433-£567 per annum) of Schedule D of the National Joint Board Agreement.

**ENGINEERING DRAUGHTSMEN (MECHANICAL).** Construction Department. (Vacancy No. 44/53.)

Senior Draughtsmen are required in the Mechanical Section of the Construction Department at North Wilford Power Station. Candidates should have experience in one or more of the following:—

(i) Design and layout of Power Station equipment, including Turbo-alternators, boiler plant, coal and ash plant, and general station auxiliaries.

(ii) H.P. and L.P. steam and feed pipework. Condensing plant and feed heating systems.

(iii) Conveyor plant, coal handling systems, and material handling of station auxiliary equipment. Salary and conditions of service will be in accordance with the National Joint Board Agreement, Grade 5 (£567-£671 per annum) and Grade 6 (£433-£567 per annum) of Schedule D, according to experience.

**ENGINEERING DRAUGHTSMEN (ELECTRICAL).** Construction Department. (Vacancy No. 61/53.)

Candidates should have experience in the preparation of layouts and diagrams for the installation of E.H.T. and L.T. switchgear, transformers, E.H.T. and L.T. cables; knowledge of protective gear systems would be an advantage.

The salary will be in accordance with Grade 5 (£567-£671 per annum) or Grade 6 (£433-£567 per annum) of Schedule D of the National Joint Board Agreement.

The above positions will be pensionable within the provisions of the British Electricity Authority and Area Boards Superannuation Scheme.

Applications should be submitted on the official form, which may be obtained from the Divisional Establishments Officer, British Electricity Authority, Barker Gate, Nottingham, and should be returned to the undersigned by the dates stated. Please quote Vacancy Number.

L. F. JEFFREY,  
Divisional Controller. 9480

### BOROUGH OF BARNES.

Temporary ARCHITECTURAL ASSISTANT, A.P.T. Grade III.

Applications are invited for the above appointment at a salary of £525×£15-£570, plus London weighting allowance.

Candidates should be good draughtsmen, and be capable of preparing plans and details for general architectural work.

Applications, giving the names of three persons to whom reference can be made, must be sent to the undersigned not later than Friday, 18th September, 1953.

W. R. SHEPHERD, A.M.I.C.E., F.R.I.C.S.,

Borough Engineer and Surveyor.

Municipal Offices, Sheen Lane, S.W.14. 9475

### EAST RIDING OF YORKSHIRE COUNTY COUNCIL.

APPOINTMENT OF ASSISTANT ARCHITECTS.

Applications are invited for the appointment of Assistant Architects on the staff of the County Architect's Department in the following salary grades of the National Scale:—

A.P.T., Grade III (£525-£570).

A.P.T., Grade IV (£555-£600).

A.P.T., Grade V (£595-£645).

A.P.T., Grade Va (£625-£685).

A.P.T., Grade VI (£670-£735).

The commencing salaries will be appropriate to professional experience and qualifications.

The appointments, which are terminable by one month's notice on either side, are subject to the provisions of the Local Government Superannuation Acts, and the successful candidates will be required to pass a medical examination.

Applications, giving particulars of age, qualifications, experience, past and present employment (with salaries), and accompanied by copies of three recent testimonials, must be received by the County Architect, County Hall, Beverley, not later than Friday, 18th September, 1953. Applicants should disclose relationship to any member or senior officer of the Council, and canvassing will be a disqualification.

THOMAS STEPHENSON,

Clerk of the Council.

County Hall, Beverley.

27th August, 1953.

### CAMEROONS DEVELOPMENT CORPORATION.

Require for service in British West Africa a DRAUGHTSMAN (Architectural), age 23-28. Must be a competent draughtsman, with a sound knowledge of building construction and detailing. He should be capable of producing working drawings from sketch designs, and a knowledge of surveying and levelling would be an advantage. Commencing salary £775, rising by annual increments to £1,175. Single quarters only for first four. First class passages are provided to and from the Cameroons for Officers and their wives, together with free furnished accommodation during residence in the Cameroons. A reasonable amount of free lighting and fuel is also provided and necessary transport. An outfit allowance of £80 is payable on first appointment. Children's allowances are payable up to a maximum of two, and in the event of married accommodation not being immediately available, a separation allowance at the rate of £200 per annum is payable. Officers are required to contribute 10 per cent. of their salary to a Provident Fund, the Corporation contributes 15 per cent. Tours of about 18 months with one week's leave on full pay for each completed month of service if the Officer is returning for further service, otherwise half this amount of leave.

The Corporation will provide a house to let, if required. The post is superannuable under the Local Government (Scotland) Act, 1937, and the successful candidate will require to pass a medical examination. Applications, with details of age, qualifications and experience, must reach the Secretary, Glenrothes Development Corporation, Glenrothes, Fife, not later than 19th September, 1953. 9496

**CRAWLEY DEVELOPMENT CORPORATION** require (a) ASSISTANT ARCHITECT Grade IV. Salary scale: £630-£650 p.a. (b) JUNIOR ASSISTANT ARCHITECT, Grade V. Salary scale: £465-£605 p.a. Contributory superannuation.

For appointment (a) applicants should be fully qualified and have good general experience in design, construction and planning.

For appointment (b) applicants should have reached the R.I.B.A. Intermediate Examination standard and have had experience in architectural practice.

Application forms from Chief Architect (Vacancy), Broadfield, Crawley, Sussex, are returnable by 18th September, 1953.

C. A. C. TURNER,

Chief Executive. 9489

POPLAR BOROUGH COUNCIL invite applications from Registered Architects for the established post of Architectural Assistant, A.P.T. V. Forms obtainable from the Borough Engineer and Surveyor, Poplar Town Hall, Bow Road, E.3. Closing date first post Monday, 28th September, 1953. 9521

HAMPSTEAD METROPOLITAN BOROUGH COUNCIL require a Chief Architectural Assistant, Associate Member of R.I.B.A. Salary £710-£835 (A.P.T. VII-VIII) plus London weighting. Age limit 40. Medical examination. No housing provided. Applications giving three referees to The Town Clerk, Town Hall, N.W.3. Closing date 25th September, 1953. 9502

### PEAK PARK PLANNING BOARD.

Applications are invited for the following appointments under N.J.C. Service Conditions:—

(i) SENIOR PLANNING ASSISTANT: Salary A.P.T. Grade VI (£670-£735), Associate Member Town Planning Institute required, preferably with additional technical qualification, car allowance payable.

(ii) JUNIOR PLANNING ASSISTANT: Salary A.P.T. Grade II (£495-£540), Intermediate Town Planning Institute Examination or equivalent standard.

Main responsibility of candidates appointed will be preparation of town maps for urban areas of north-west Derbyshire outside National Park but adjacent thereto: previous experience of town map work essential.

Apply on forms obtainable, together with further particulars, from undersigned: forms to be returned by 29th September.

Bath Street,

Bakewell, Derbyshire.

A. L. OLDACRE,  
Planning Officer. 9503

### COUNTY OF ARMAGH EDUCATION COMMITTEE.

APPOINTMENT OF ARCHITECTURAL ASSISTANTS.

Applications are invited for the following appointments in the Architect's Department of the above Committee:—

(a) ASSISTANT ARCHITECT. Salary scale: £655-£970 per annum. Candidates should be Registered Architects, and preferably Associate Members of the Royal Institute of British Architects.

(b) ARCHITECTURAL ASSISTANT. Salary scale: £400-£600 per annum. Candidates should have passed the Intermediate Examination of the R.I.B.A.

The provisions of the Local Government Superannuation Act (N.I.), 1950, will apply.

Further particulars and forms of application may be obtained from the undersigned, to whom applications must be returned not later than the 30th September, 1953.

W. H. DAWSON,

Director of Education.

Education Office, Courthouse, Armagh.

2nd September, 1953. 9499

AIR MINISTRY WORKS DEPARTMENT requires ARCHITECTURAL ASSISTANTS in Design Branch. Applicants should be experienced in the planning and preparation of working drawings and details for permanent and semi-permanent buildings. Professional qualifications, an advantage. Vacancies mainly London, few Provinces. Appointments carry liability for overseas service, for which allowances are payable. Salaries: £623 to £733 p.a., starting pay dependent upon age, quals., and experience. Extra duty allowance payable. Reasonable prospects of promotion. Post temporary, but many have long term possibilities, and competitions are held periodically to fill established vacancies. Apply, quoting BB, and stating age, quals., and previous appointments, giving dates and stating type of work done, to Air Ministry, S.2(h)/NA, Cornwall House, Stamford Street, London, S.E.1. 9498

### CITY OF SHEFFIELD.

CITY ARCHITECT'S DEPARTMENT.

APPOINTMENT OF CHIEF QUANTITY SURVEYOR.

Scale: £1,150-£1,350.

Applications are invited from Chartered Quantity Surveyors for this appointment on the staff of the City Architect (Mr. J. L. Womersley, A.R.I.B.A., A.M.T.P.I.).

The City's building programme comprises mixed developments of houses and flats including roads and sewers, the redevelopment of outworn central areas, including multi-storey flats, schools, and the general building works of all Committees.

Candidates should possess a thorough knowledge of Building Contract procedure, sound administrative ability, and considerable experience of handling large scale contracts.

The person appointed will be required to take charge of the Quantity Surveying Section, and will be directly responsible to the City Architect and his Deputy for the Quantity Surveying work of the Department.

The post is superannuable and subject to medical examination.

Applications, stating age, education and training qualifications, present and past appointments (with dates and salaries), experience, and the names and addresses of two referees, should reach me by the 24th September, 1953.

JOHN HEYS,

Town Clerk. 9524

The Town Hall, Sheffield, 1.



CITY OF SHEFFIELD.

CITY ARCHITECT'S DEPARTMENT.

Applications are invited for the following two principal architectural appointments on the revised establishment of this Department under the City Architect (Mr. J. L. Womersley, A.R.I.B.A., A.M.T.P.I.) and his Deputy. Both these appointments offer considerable scope to enthusiastic and progressive architects possessing the requisite qualifications, design ability and experience.

(a) CHIEF ASSISTANT ARCHITECT (HOUSING). Scale: £1,150-£1,350.

The City's housing programme consists of mixed developments of houses and flats in the outer and middle areas, and the redevelopment of outworn and densely populated central areas. The person appointed, who should be a Member of the R.I.B.A. and the T.P.I., will be required to take charge of the "Housing" Section of the Department.

(b) CHIEF ASSISTANT ARCHITECT (EDUCATION AND GENERAL). Scale: £1,150-£1,350.

The City has an extensive school building programme, and the "General" work, which is extremely varied, is expanding with the freeing of restrictions. The person appointed, who should be a Member of the R.I.B.A., will be required to take charge of the "Education and General" Section of the Department.

The posts are superannuable, and subject to medical examination.

Applications, stating age, education and training, qualifications, present and past appointments (with dates and salaries), experience, and the names and addresses of two referees, should reach me by the 24th September, 1953.

JOHN HEYS.

Town Clerk.

The Town Hall, Sheffield, 1.

9525

MONMOUTHSHIRE COUNTY COUNCIL. APPOINTMENT OF ARCHITECTURAL STAFF.

Applications are invited for the following posts in the County Architect's Department under N.J.C. Conditions:—

ONE SENIOR ARCHITECTURAL ASSISTANT. Salary: £760-£835 (A.P.T. VIII).

ONE ARCHITECTURAL ASSISTANT. Salary: £710-£785 (A.P.T. VII).

ONE ARCHITECTURAL ASSISTANT. Salary: £555-£600 (A.P.T. IV).

ONE ARCHITECTURAL ASSISTANT. Salary: £525-£570 (A.P.T. III).

Forms of application, particulars of posts and Conditions of Service, can be obtained from the undersigned. Applications, together with copies of three testimonials, must be forwarded to the County Architect, Queen's Hill, Newport, Mon., not later than 26th September, 1953.

VERNON LAWRENCE.

Clerk of the Council.

County Hall, Newport, Mon.

9490

CITY OF NOTTINGHAM.

ASSISTANT QUANTITY SURVEYOR, GRADE A.P.T. VII (£710 to £785 per annum).

Applications are invited for the above position from persons with ability to carry out all duties, including the preparation of Bills of Quantities, Estimates, Final Accounts, and the supervision of Prime Cost Contracts for important building and civil engineering schemes. They should be Chartered Surveyors, qualified by examinations in Quantities or who have had pre-war experience in a similar capacity.

The appointment is subject to the provisions of the Local Government Superannuation Act, 1937, as amended by Part XI of the Nottingham Corporation Act, 1952.

Applications are to be made on forms to be obtained from R. M. Finch, Esq., O.B.E., M.I.C.E., City Engineer and Surveyor, Guildhall, Nottingham, to whom they must be returned not later than Wednesday, 16th September, 1953.

T. J. OWEN.

Town Clerk.

Guildhall, Nottingham.

9497

Competition

6 lines or under, 12s. 6d.; each additional line, 2s.

COMPETITION IN EDINBURGH.

The Church of Scotland Home Board invites Architects resident in Scotland to submit designs in competition for a church and ancillary buildings for a site at Sighthill, Edinburgh.

There will be five prizes: £750, £450, £300, £200 and £100.

The Assessors will be: Professor Robert H. Matthew, C.B.E., M.A., A.R.I.B.A., A.R.I.A.S., Mr. Harry Taylor, A.R.I.B.A., A.R.I.A.S., Architect to the Church of Scotland Home Board, and the Rev. Professor J. G. Riddell, D.D., Convener of the Church of Scotland National Church Extension Committee.

The closing date for submission of designs is 12 noon, Saturday, 30th January, 1954, and the last date for questions is Thursday, 15th October, 1953.

Competition conditions and a plan of the site may be obtained from the Rev. Ivan F. Tibbs, M.A., The Church of Scotland Offices, 232, St. Vincent Street, Glasgow, C.2, on payment of a deposit of £2 2s., which will be returned on receipt of a bona fide design or on the return of the competition documents at least four weeks before the last day for the submission of designs. 9474

Architectural Appointments Vacant

4 lines or under, 7s. 6d.; each additional line, 2s.

The engagement of persons answering these advertisements must be made through a Local Office of the Ministry of Labour or a Scheduled Employment Agency if the applicant is a man aged 18-64 inclusive or a woman aged 18-59 inclusive unless he or she is, or the employment, is excepted from the provisions of the Notification of Vacancies Order, 1952.

ASSISTANT required for large general Architectural Practice with offices in Maidenhead. Some experience in specification writing essential. Salary £300 to £500, according to experience. Box 9933.

ARCHITECTURAL ASSISTANTS—Simon-Carves Ltd., have several vacancies for architectural assistants interested in reinforced concrete industrial structures. The main fields covered are coal preparation plant, coke ovens, chemical plant, and power stations. The work offers excellent experience and good scope. A Pension Fund and D.O. Bonus Scheme are in operation. Apply, giving age and details of experience, to Staff and Training Division (Ref. T.E. 12), Simon-Carves Ltd., Cheadle Heath, Stockport. 9443

ASSISTANTS required in Architect's Department of large commercial organisation. Sound all round training in the profession essential including supervision of work. Excellent opportunities for men of initiative. Apply in writing, giving details of experience, age, and salary required, to Box 3675, c/o Foster, Turner & Everetts, 11, Old Jewry, London, E.C.2. 9453

ARCHITECTURAL DRAUGHTSMAN required in Multiple Shop Company. Experience in 4-in. and 3-in. working drawings, able to work on own initiative. Five-day week, staff canteen, superannuation scheme, permanent position after qualifying period. Please reply stating age, salary required, when available to Box 9469.

ARCHITECTURAL ASSISTANT, at least Inter. R.I.B.A. standard, required for busy county practice. Write stating age, experience and salary required, to Edwin H. Earp & Badger, L./A.R.I.B.A., Scholars Lane, Stratford-on-Avon. 9462

ARCHITECTS in South-East Scotland require ASSISTANT, qualified or final standard. Apply, stating age, experience, and salary required. Box 9472.

ARCHITECTURAL ASSISTANT required immediately for London Brewery. Must be good Draughtsman, with sound knowledge of construction. Please reply, giving details of experience and training, with salary required, to Box 9492.

RESIDENT ARCHITECT required for Eastern Region British Railways. Successful applicant will be attached to the Architect's Office at King's Cross, and required to supervise contract work in the Peterborough, Newark and Lincoln areas. Applicants should be qualified and have had practical experience and should reside within reach of the areas mentioned. Salary: £612 5s. per annum. Free residential railway travelling within specified limits and other reduced rate travelling facilities after qualifying period of service. Permanency to suitable applicant after probationary period. Five-day week. Apply in writing, giving full particulars as to qualifications, experience, etc., to the Civil Engineer, Eastern Region, British Railways, King's Cross Station, N.1. 9484

ASSISTANT required in Surveyors' Department of London Brewery, age 25-30. Knowledge of Estimating, Specifications and supervision, able to drive car. Superannuation scheme. Reply, giving details of experience, with salary required. Box 9493.

TIMBER DEVELOPMENT ASSOCIATION. ARCHITECT.

APPLICATIONS are invited for the appointment of Architect in the Research and Design Department of the Timber Development Association at a commencing salary of £1,000 per annum. Applicants should be Fellows or Associates of the Royal Institute of British Architects, should have a keen sense of contemporary design and an aptitude for research into user requirements and the development of new techniques. Experience or special interest in educational activities would be an advantage. The successful candidate will be required to join the Association's Staff Superannuation Scheme, if invited to do so. Applications, stating age, qualifications and experience, together with names of three persons to whom reference could be made, should be addressed to the Director, Timber Development Association, Limited, 21, College Hill, London, E.C.4, to be received not later than 1st October, 1953. 9494

ARCHITECT'S ASSISTANT required in London office. Interesting and varied practice, including housing, licensed premises, hospital, factory and laboratory works. Passed R.I.B.A. Intermediate essential. Five-day week, staff profit sharing scheme. Salary: £494 per annum. Applications by letter only, giving particulars of training and experience, to: Stewart & Hendry, F./A.R.I.B.A., A.M.T.P.I., 90, Fenchurch Street, London, E.C.3. 9495

VACANCY arises for Articled Pupil (Architectural or Building Surveying) in City firm. Box 9468.

CLIFFORD TEE & GALE F/R.I.B.A. require Architectural Assistant for industrial work in their office at 43, Frederick Road, Birmingham, 15. Five-day week. 9511

ARCHITECTS' ASSISTANT required, at least Intermediate R.I.B.A. Standard and office trained. Apply stating experience and salary required to Fairbrother, Hall & Hedges, 2, Birley Street, Blackpool. 9510

WATES LTD. invite applications from newly qualified school-trained A.R.I.B.A.'s for a permanent position in their Architects' Department to assist in the design and detailing of housing work of all kinds. Applicants must have completed their National Service. Commencing salary £450 per annum. Applications in writing, to The Architect, Wates Ltd., 1258/60, London Road, Norbury, S.W.16. 9512

ARCHITECTURAL ASSISTANT aged 28/40 years, experienced in design of industrial premises, all stages, including Surveys, Supervision of works in progress, Contractors' accounts. State salary required, qualifications, earliest time available. Walker & Collinson, Architects & Surveyors, 12, The Exchange, Bradford, Yorks. 9516

ARCHITECTURAL ASSISTANT, Inter. R.I.B.A. standard, required in Reading office of Chartered Architects. Applicant must be capable of preparing working drawings, details and specifications. Salary: £350 to £500. All details to Box 9526.

ARCHITECTURAL ASSISTANTS immediately required by London Architects for large contemporary design scheme. Temporary staff. Salary: £12-£15 per week. Box 9522.

BUSY contemporary Practice, S.W.1, requires experienced, young ASSISTANT, for varied work on housing, shops and industrial schemes, including perspectives and colour work. Salary according to ability. Telephone TAT. 3824-5. 9523

Architectural Appointments Wanted

ASSISTANT (25), Inter., School Trained to final yr., some office exp., N/S R.E. Wks., seeks post, small, prog. office, North, Midlands or Cambridge. Box 759.

SCULPTOR and Architect's Assistant, Final standard, 54 years' varied experience, requires position where integrity, enthusiasm and interest are not stifled. Box 9422.

CHARTERED ARCHITECT (40), requires responsible appointment with view to Partnership. Over 20 years' extensive experience, S. or S.E. Coast area preferred, or London. Some capital available. Box 9501.

ARCHITECTURAL ASSISTANT requires position in London office. Neat draughtsman, with varied experience. Box 760.

REMUNERATIVE post required in London by young Associate; married; five years' school training, three years' experience in small practices engaged with general work and private estate development. Box 752.

ARCHITECT (34) requires executive or appointment leading Junior Partnership in Africa. Full experience, design, payments, variations and physical building in Africa. Present salary £1,300 and privileges. Available for interview in United Kingdom. Box 9507.

SENIOR ASSISTANT, A.R.I.B.A., school trained, 5 years' post-graduate work specialising in large scale industrial and engineering projects involving R.C. and steel structures, seeks appointment of responsibility. Conversant with all phases of work including site supervision and co-ordination of specialist Sub-Contractors. Salary £750 p.a. Box 9512.

A.R.I.B.A. (38) seeks position as CHIEF or SENIOR ASSISTANT in London. 6 years' pre-war and 7 years' post-war experience in eminent London Architects' offices. Minimum salary £900 per annum. Box 9514.

SENIOR ARCHITECTURAL ASSISTANT seeks new appointment in London. Large experience in preparation of contract particulars and supervision of works and schools, housing blocks, adaptation of buildings for flats, factory buildings, welfare blocks. Large knowledge of L.C.C. Building Acts and Town and Country Planning Act, also varied experience in claims on the War Damage Commission. Box 9517.

SENIOR ASSISTANT school trained, 23 years' experience in the design and construction of domestic buildings, large industrial projects, factories, research laboratories, etc., able to control works throughout, supervision of draughtsmen and site works, seeks responsible senior position. Salary £750. Box 743.

**SENIOR ASSISTANT** seeks Position. Experienced private and local authority housing, surveying, levelling, farm buildings, conversions, detailing, specifications, supervision, and final accounts. Used to taking full control. Own car. Box 9515.

### Other Appointments Vacant

4 lines or under, 7s. 6d.; each additional line, 2s.

The engagement of persons answering these advertisements must be made through a Local Office of the Ministry of Labour or a Scheduled Employment Agency if the applicant is a man aged 18-64 inclusive or a woman aged 18-59 inclusive unless he or she is the employment, is excepted from the provisions of the Notification of Vacancies Order, 1952.

**SENIOR DRAUGHTSMAN** required for Architects with extensive general practice, chiefly commercial, in all parts of the United Kingdom, with offices in Maidenhead. Reply, giving full details of experience and salary required, to Box 9477.

**QUANTITY SURVEYORS' ASSISTANT** required by Architects with extensive general practice, chiefly commercial, in all parts of the United Kingdom, with offices in Maidenhead. Reply, giving full details of experience and salary required, to Box 9478.

**AUTHOR and Editor** of technical articles on Architecture, the Building Industry, and on the products manufactured for the industry required for a new technical section of an Architectural Magazine. Post could be held on a part-time basis. Applicants should have a good general knowledge of the building industry, the ability to write, and be capable of judging what information the architect-reader requires. Professional architectural qualifications desirable, but not essential. Salary according to experience. Apply in writing only, to the Organising Secretary, The Architectural Press, Ltd., 9-13, Queen Anne's Gate, London, S.W.1. 9487

### TECHNICAL ASSISTANT.

**BIRMINGHAM CO-OPERATIVE SOCIETY** requires the services of a Technical Assistant in the Works Department to work under the direct supervision of Mr. B. K. Lewis, A.R.I.B.A., the Society's Architect and Works Manager. Applicants should have a sound knowledge of building construction and shop-fitting, be able to prepare a in. scale plans, estimates and builders' quantities, and carry out surveys of properties for alteration and maintenance, preparing reports thereon as may be required. Superannuation fund (with transfer clause). This position is a senior one and the salary will be commensurate with it. Applications, stating age, qualifications, and previous experience, should be addressed to Personnel Officer, Birmingham Co-operative Society, Ltd., High Street, Birmingham, 4, to arrive not later than 19th September. 9485

**DRAUGHTSMAN** with Engineering and Building experience. Capable of preparing working drawings from Architects' sketches. Able to work on own initiative. Salary roughly £400-£500 per annum, under 28 years age preferred. Please send details of age, qualifications and experience to: Personnel Manager, Messrs. Schweppes Ltd., Schweppes House, 1/4, Connaught Place, W.2. 9505

**THE WALPAMUR CO., LTD.**, has a vacancy in the Interior Design Department at its London office for a **JUNIOR ASSISTANT (I.B.D.)**. Inter. standard or equivalent. Apply, stating age, education and experience, to: The Secretary, The Walpamur Co., Ltd., Darwen. 9500

### Partnerships

6 lines or under, 12s. 6d.; each additional line, 2s.

**ASSOCIATE** with small West End practice, in excellent position, desires to enter into Partnership with young Architect with varied experience. Contemporary outlook and some capital or clients. Box 9504.

**A YOUNG** school trained **ASSOCIATE** is required as a Partner in a growing North Wales practice. Applicants should have contemporary outlook, and preferably experience of private practice. Preliminary details of professional and personal background are asked for in the first instance. Box 9518.

### For Sale or Wanted

4 lines or under, 7s. 6d.; each additional line, 2s.

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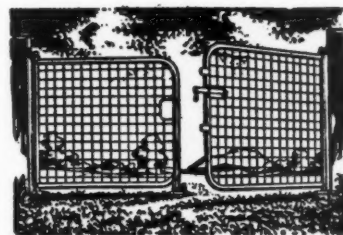
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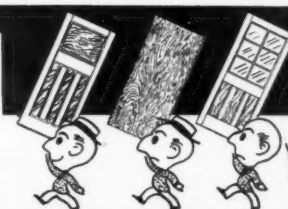
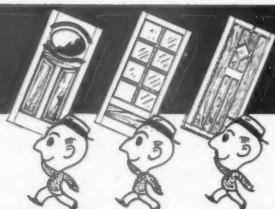
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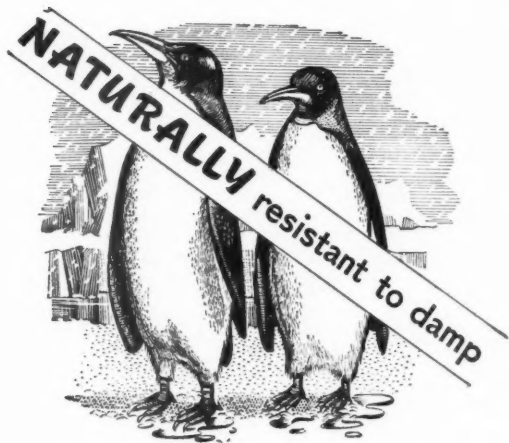
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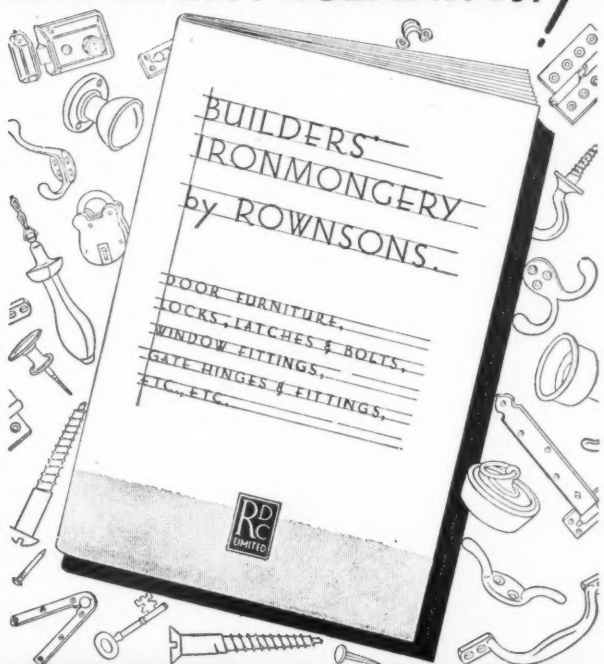
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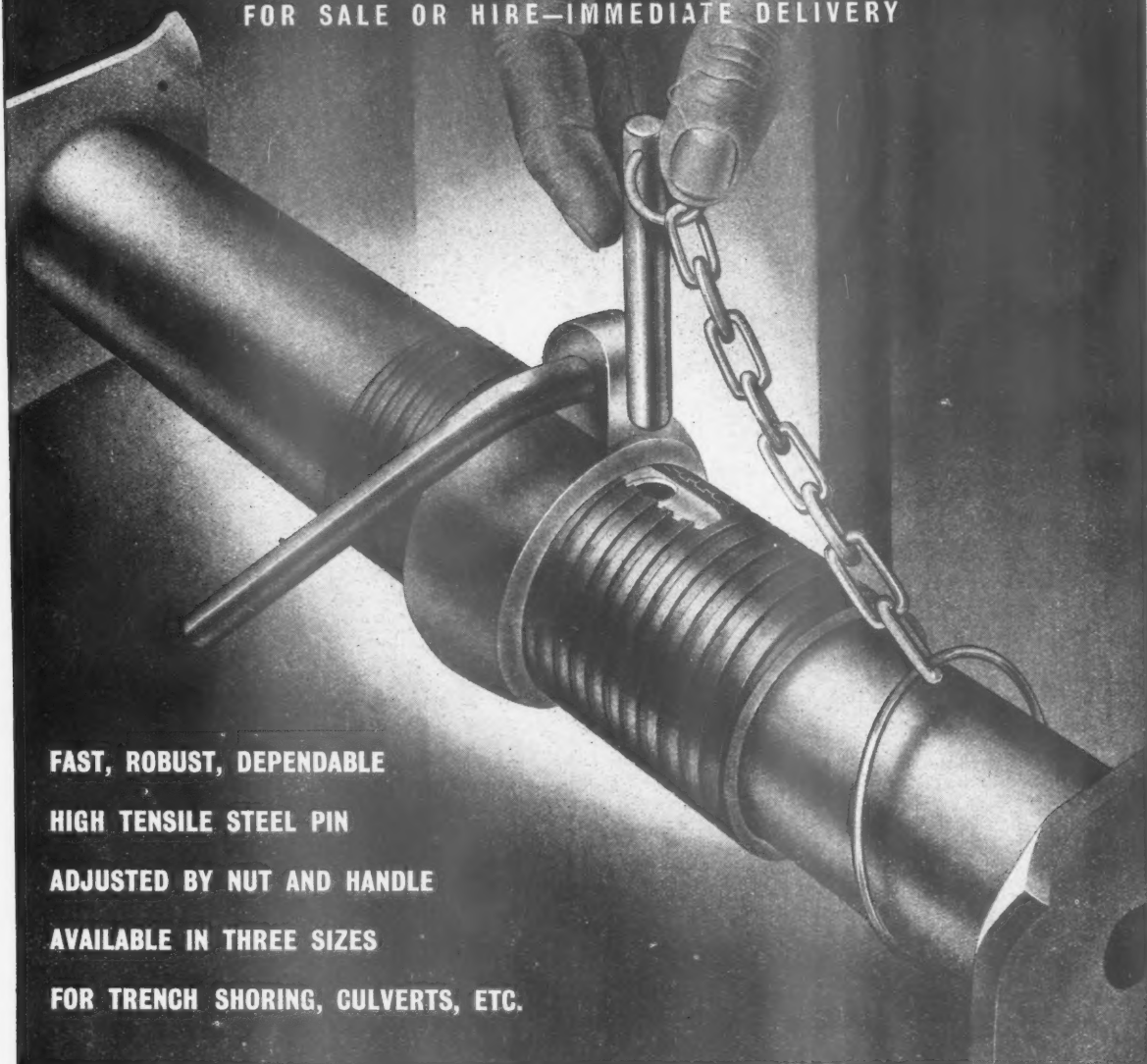
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